

Construction Traffic Management Plan


Northern Dive Site Operations

Line-wide Works Contract Sydney Metro City & Southwest

Project number: C600
Document number: SMCSWLWC-SYC-DCH-TF-PLN-002505
Revision date: 18 October 2023
Revision: 5

Document Approval

Rev.	Date	Prepared by	Reviewed by	Recommended by	Approved by	Remarks
A	13 Mar 2020	Mong Sim	Kia Arbabi	Simon Tibbett	Scott Hunter	Initial submittal.
B	24 Apr 2020	Mong Sim	Jason Bitmanis	Simon Tibbett	Scott Hunter	General comments from stakeholder.
C	8 May 2020	Mong Sim	Jason Bitmanis	Simon Tibbett	Scott Hunter	General comments from stakeholder.
0	26 May 2020	Mong Sim	Jason Bitmanis	Simon Tibbett	Scott Hunter	Approval.
1	3 July 2020	Mong Sim	Jason Bitmanis	Simon Tibbett	Scott Hunter	Additional comments.
2	3 Mar 2021	Mong Sim	Jason Bitmanis	Simon Tibbett	Scott Hunter	Usage of an existing light vehicles gate at Mowbray Road.
3	11 Aug 2022	Mong Sim	Omar Alfahem	Kia Arbabi	Scott Hunter	Appendix E added for utilities work at Mowbray Road
4	12 Sept 2023	Matthew Keswick	Jason Austin	Kia Arbabi	Adrian Wuillemin	Appendix F added for Redundant Shed Removal
5	18 October 2023	Matthew Keswick	Jason Austin	Kia Arbabi	Adrian Wuillemin	Appendix F redrafted in response to comments from stakeholders

Signature:	<i>M Keswick</i>	<i>J Austin</i>	<i>Kia Arbabi</i>		
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Details of Revision Amendments

Document Control

The Project Director is responsible for ensuring that this plan is reviewed and approved. The Project Traffic Manager is responsible for updating this plan to reflect changes to legal and other requirements.

Amendments

Any revisions or amendments must be approved by the Project Director and/or client before being distributed/implemented.

Revision Details

Revision	Details
A	Issued to for stakeholder review.
B	Comments from SCO, Transport NSW, Sydney Metro and Council incorporated into the CTMP.
C	Revised start date and minor revision to Section 3.6. Removal of 3D rendering diagram per SM request.
0	No further comments from TfNSW provided all other stakeholders comments are satisfactorily addressed.
1	Sydney Metro additional comments.
2	Reopening of an existing gate (closest to Pacific Highway) at Mowbray Road to segregate light and heavy vehicles from using the same entry and exit gate currently used. The proposed gate usage is subject to TfNSW instruction and/or conditions. Update on Figure 2, reworking on Section 2.1, Section 3.2., Figure 6A, Table 1.
3	Appendix E added to include utilities work at Mowbray Road.
4	Appendix F added to include shed dismantlement along Nelson Street and Pacific Highway
5	Appendix F redrafted in response to comments from stakeholders

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1. PART A – Management Systems, Compliance and CTMP Overview

1.1. Structure of this Plan

This Construction Traffic Management Plan (CTMP) describes Systems Connect traffic management planning and compliance during the construction stage for the Sydney Metro City & Southwest.

Part A: Overview	This section clearly defines: <ul style="list-style-type: none">• Project Overview, Objectives, Management and Compliance• Overall project summary and overview
Part B: Implementation	This section outlines in detail the key aspects for Traffic Management on the Project including: <ul style="list-style-type: none">• Implementation Details• Traffic Impact Assessment• Transport Management• Communications
Part C: Appendices	This section provides the following Appendices: <ul style="list-style-type: none">• Design/Staging drawings• Construction vehicles route• Copy of correspondence• Compliance matrix

This Construction and Traffic Management Plan (CTMP) forms part of the Systems Connect Integrated Management Systems.

1.2. Management and Planning Strategy

This CTMP dictates the overall traffic management plan including resources, processes and procedures during construction of Sydney Metro Chatswood to Bankstown works.

This Plan aims to address the following objectives:

- Local road staging and traffic management plan
- Obtaining relevant approvals, whether from Local Council, Transport for NSW, Customer Journey Planning (CJP), Sydney Metro
- Specific community / stakeholder consultation process and community relations strategies for managing changed traffic conditions
- Potential road network impacts and the mitigation and management of them
- Auditing, inspections and monitoring the road network
- Fulfill the requirements of Principal's G10 Specification – Traffic and Transport Management
- Meet the contractual requirements
- Management of incidents
- Provide and facilitate a mechanism for the monitoring, ongoing regular review and updating of this CTMP.

1.3. Compliance

This CTMP has been developed to address Planning Approval and Frameworks requirements as applicable by location. They are:

- Critical State Significant Infrastructure (CSSI 7400) and Revised Environment Mitigation Measure
- Critical State Significant Infrastructure (CSSI 8256) and Revised Environment Mitigation Measure
- Sydney Metro City and Southwest Construction Environment Management Framework
- Sydney Metro City and Southwest Construction Traffic Management Framework

A Planning Approval and REMM compliance matrix is provided in Appendix D

1.4. Relevant Legislation

The key legislation relevant to traffic management includes:

- Environmental Planning and Assessment Act (EPA) 1979 Act
- The Roads Act 1993.
- Heavy Vehicle National Law 2014
- Work Health and Safety (WHS) Act 2011
- Principal's General Specification G10 – Traffic and Transport Management
- Traffic Control at Worksites Manual
- Relevant Australian Standards (AS) and Ausroads Guidelines

1.5. LW Project Overview and Scope

Line-wide Works (LW) is delivered by Systems Connect, a CPB Contractors and UGL Engineering Joint Venture. Systems Connect is delivering LW in four distinct portions as follows, and as described in detail in Section 1.3.

- Portion 1 – SMTF (Tallawong) expansion works
- Portion 2 – SMTF South (Marrickville) stabling yard
- Portion 3 - Chatswood to Sydenham works
- Portion 4 – Sydenham to Bankstown works

The Sydney Metro City & Southwest (SMCSW) project will extend the current Sydney Metro Northwest which stops at Chatswood, to the CBD and to Bankstown.

The SMCSW project is being delivered through a series of contracts for the tunnels, stations, line-wide infrastructure and systems.

Line-Wide Works to be constructed by Systems Connect include:

- Tunnel works between Chatswood and Sydenham, comprising:
 - Tunnel track slab and rails;
 - High voltage reticulation, traction power and power control systems;
 - Earthing and bonding, electrolysis control and lightening protection measures;
 - Tunnel ventilation system;
 - Tunnel mechanical and electrical services;
- Northern Dive track slab and rails;
- Northern Dive Service Building works – Chatswood;
- Artarmon Substation Service Building works;
- Southern Dive Service Building works - Sydenham;
- Station civil works between Chatswood and Sydenham;
- Extension of the existing Sydney Metro Trains Facility – Tallawong;
- Construction of a new Sydney Metro Trains Facility (South) – Sydenham;
- Northern Connection works, tying the new Metro lines into the T1 North Shore Line – Chatswood.

LW is a key component of the SMCSW project, with works taking place over the full length of the project as shown in Figure 1 between Chatswood and Bankstown.

Sydney Metro City & Southwest Indicative Linewise Scope

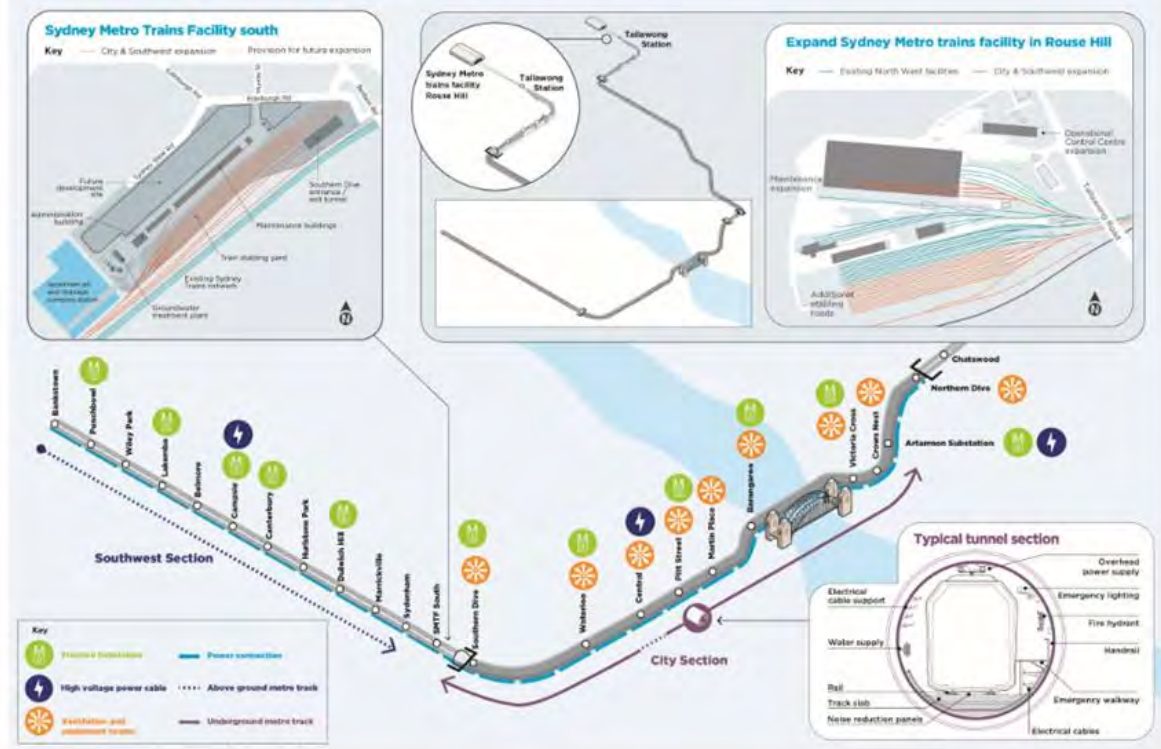


Figure 1. Line-wide Works Line Diagram (Latest diagram)

2. PART B – Implementation

2.1. Northern Dive building site description

The northern dive building site is located approximately within the perimeter of Nelson Street - Pacific Highway – Mowbray Road and adjacent to the T1 Northshore line rail corridor at Chatswood. The northern dive building site is shown on Figure 2.

The northern dive building site works are listed below and not limited to:

- a service building construction
- water storage tank building including a pump room
- landscaping and other civil/mechanical works (fencing, plumbing, etc)
- removal of temporary signalised access off Mowbray Road (the signalised intersection maybe retained as a permanent asset, asset owner to advise final option)
- new driveway off Nelson Street
- logistics delivery point for tunnel fitting materials, equipment and components

During metro tunnel fitout works the site will operate as a logistics and maintenance depot, northern tunnel access point, and site compound with welfare facilities for construction and maintenance personnel. Typical deliveries will include rail, concrete, precast elements and mechanical and electrical tunnel components.

Access into the work site is via an existing angled entry and existing signalised exit (intersection) off Mowbray Road only. There is an existing gate off Pacific Highway which is not to be used as general construction access. The gate off Pacific Highway is unlikely to be used through the course of construction phase. Any use of this gate requires an agreement by Customer Journey Planning (CJP) and/or Transport for NSW. An existing gate off Mowbray Road that is not currently used at Mowbray Road is proposed to be reopened as shown on Figure 2. The reopening of this gate allows separation of light vehicles from heavy vehicles that both currently share the same entry and exit. The proposed gate is both as an entry and exit for light vehicles only. The gate will be used between 20 to 30 light (office staff) vehicles. The movement is mostly between 6am – 9am and 4pm – 6pm.

Northern dive building construction site work will begin from November 2020 till mid 2023.

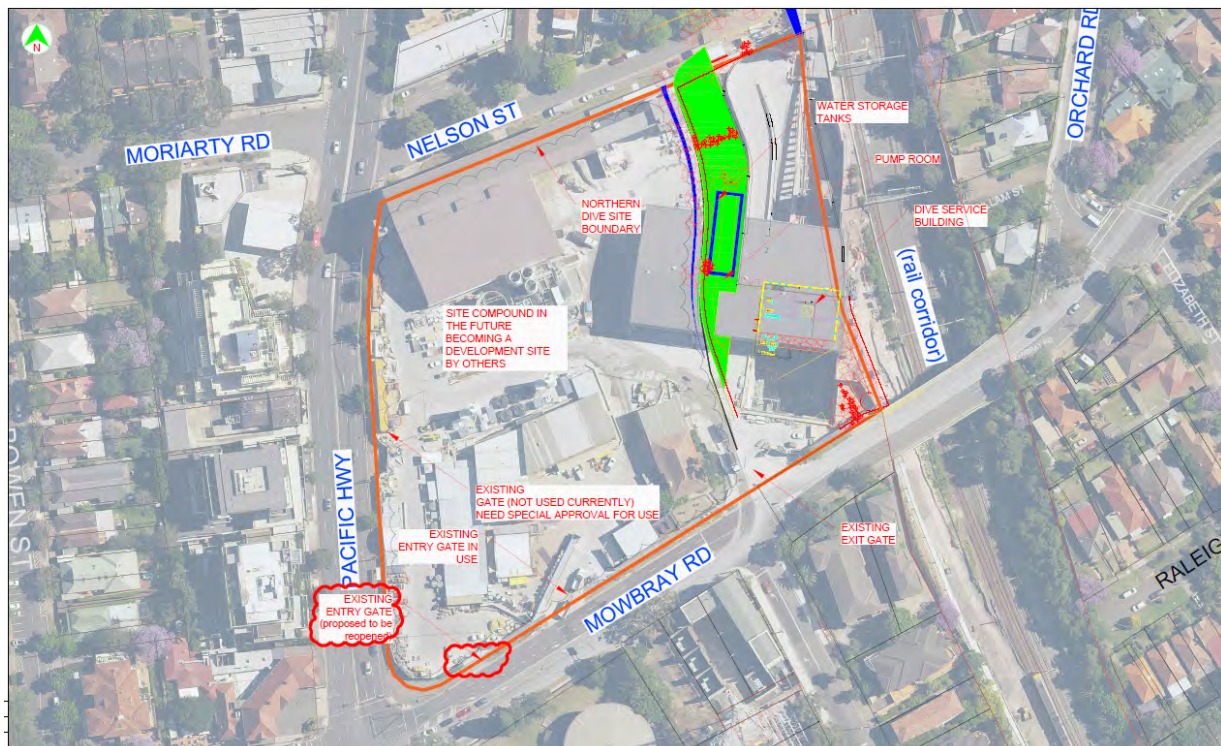


Figure 2 – Locality map of the northern dive works. Refer to Appendix A.

(Figure 3 removed)

All existing temporary traffic signs, median, pavement marking installed previous by contractor will be retained.

3. Traffic and Transport Management

3.1. Parking

Pacific Highway between Fullers Road and Mowbray Road is within the Sydney Clearways Program that was rolled out in stages from circa 2018. The clearway is signposted Monday – Friday from 6am to 7pm, clearway from 9am to 6pm Saturday, Sunday and public holiday. This section along Pacific Highway southbound is signposted “No Parking” outside Clearway hours.

Limited construction staff only parking is available within the compound boundary. Construction staff are expected to take advantage of the site close proximity to Chatswood and Artarmon train station to use the train as the chosen mode of transportation to and from work. Construction staff are not recommended to use side streets parking in the area.

3.2. Pedestrians/Cyclists

Footpath is not impacted nor closed at anything stage during the work. Existing and new pedestrian guide signs are maintained/added till end of construction. There are no dedicated cycle routes along the area and thus no immediate impact. Cyclists to adhere to standard road rules. Removal of the high fencing may require temporary footpath closure during the endstate. Details of removal is yet to be finalised depending on final design phase that is on going at the time of CTMP development.



Figure 4 – Existing pedestrian management signs to be maintained (looking east from Mowbray Road)



Figure 5 – Existing pedestrian management signs to be maintained during construction (looking east from Mowbray Road)



Figure 6 – Existing entry and exit gate off Mowbray Road. (looking towards west from Mowbray Road)



Figure 6A – Proposed reopening of the existing gate at Mowbray Road with pedestrian warning signs on both sides of the driveway

3.3. Businesses / Residents Access

The work site is not adjacent to any immediate businesses nor residential establishments.

3.4. Bus Operations

The closest bus stop within the perimeter of the work area along Pacific Highway southbound bus stop # 206747 located between Nelson Street and Mowbray Road. The bus stop is not impacted during the operations of the building site.



Figure 7 – Bus stop # 206747 along Pacific Highway. (looking south from Pacific highway)

3.5. Emergency Services

Emergency services are not impacted from the works as there are no road closures in place during the operations of the building site.

3.6. Construction Vehicles Route

Routes for construction vehicles nominated are the most direct to the site. Building and tunnels materials delivery route by 12m long or greater vehicles (see Figure 4) is via all the major arterial roads – Pacific Highway and M2.

Concrete delivery route (Figure 5) is originating from Artamon industrial area (for instance Boral concrete plant located at 88 Reserve Road, Artamon). The nominated delivery route is within an industrial route via Campbell Street - Pacific Highway – Mowbray Road - with no usage of local residential roads. Return route is the same except empty concrete trucks turn off at Dickson Avenue to take advantage of the signalised intersection of Dickson Avenue and Reserve Road for a safer and controlled merging.



Name	Role	Contact Details
Carl Mella	Transport for NSW – Integration Leader	13 22 13
Jake Coles	Sydney Coordination Office - Operations Manager – CBD	0466 454 819
Stephen Brown	Sydney Coordination Office - Precinct Project Manager	0457 809 028
Phil Brogan / Ken Hind	Sydney Metro – Traffic Advisors	0401 719 632 0416 797 029
Gordon Farelly	Willoughby Council – Traffic & Transport Team Leader	02 9707 9000
Matt Billings	Systems Connect – Environment Manager	0428 781 599
Simon Tibbett	Systems Connect – Project Manager	0457 761 648
Craig Godwin	Systems Connect – Safety Manager	0458 498 107
Svetlana Paunovic	Systems Connect – Community Manager	0438 540 245
Scott Francis	Systems Connect – Superintendent	0429 901 489
Mong Sim	Systems Connect – Traffic Engineer	0448 378 883

5. Communications and Community Strategy

Systems Connect will meet the reasonable needs and desires of the community for information on any changed traffic conditions, cyclist and pedestrian impacts and property access arrangement. Systems Connect will ensure that the public and other key stakeholders are informed of planned traffic arrangements, including any activities which may result in delays.

Communications, consultation and the dissemination of information associated with traffic and access will be undertaken as outlined in this section.

The aim of consultation and broad communication on traffic and access matters is to:

- Facilitate community feedback regarding traffic issues
- Recommend alternative and appropriate travel patterns during periods of change
- Manage traffic impacts to protect affected residential and business amenity
- Provide timely, accurate and comprehensive traffic information using all available media to inform road users and the community of the project's traffic impact mitigation measures.

Ongoing consultation with stakeholders will ensure that effective traffic management measures are developed and implemented to minimise disruption and inconvenience.

Systems Connect will coordinate engagement with Sydney Metro and the members of the TTLG to enable the local community and other stakeholders to receive timely and accurate information associated traffic and transport issues.

Tool	Purpose	Frequency
Traffic alert emails	Email alerts to Transport for NSW , Transport Management Centre, Council, transport operators and emergency services to advise of major traffic changes including road or lane closures and detours, incidents or undue congestion	5 business days prior to changes if applicable As soon as practicable following incidents or undue congestion
Advertisements	To inform of significant traffic changes, detours and traffic disruptions as required to comply with approvals; in local newspapers, radio and/or project website	5 business days prior to changes
Letterbox notifications	Notification letters to inform local residents and businesses potentially affected by planned traffic changes	7 business days prior to changes
Community emails	To inform and update the community of project progress, milestones, activities planned for the following	As required

Tool	Purpose	Frequency
	month, current and upcoming traffic changes	
Community information line	Information to the project details with message service via an 1800 number	As required
TfNSW Sydney Metro website	Systems Connect will provide information in electronic format suitable to be uploaded onto the TfNSW Sydney Metro website, including copies of advertisements, traffic alerts, notification letters and other public material related to the works	As required
Systems Connect website	Information about the northern dive building site construction activities will be placed on the Systems Connect website including information about traffic changes, and executive summaries of publicly available reports relating to the project activities.	As required

The table above provides a guide to inform the community of changes to road and traffic conditions. It also provides a summary of the purpose and frequency of each method of communication.

6. Working Hours

The standard working hours 7am – 6pm on weekdays and 8am – 1pm on Saturdays. Some activities will need to be undertaken outside of these hours. Refer to out of hour application for additional details as required.

Construction Activity	Construction Hours / Comments
Standard construction hours	Monday to Friday: 7am – 6pm Saturdays: 8am to 1pm Sundays & Public Holidays: No work
Standard out of hours	Saturday afternoons: 1pm – 5pm Sundays: 8am – 5pm
24 hour construction where applicable	(Refer to condition E48)

7. Manage Emergencies

Systems Connect acknowledge the inevitable nature of emergencies and their potentially significant social, economic and environmental consequences. Accordingly, we are aware that the NSW Government has enacted the State Emergency & Rescue Management Act 1989 to support emergency management activities.

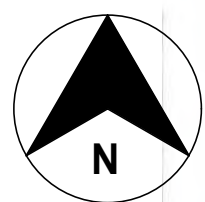
In NSW, the agencies primarily responsible for controlling hazards/emergencies are:

Unplanned Incident Agency Responsibility	
Law Enforcement / Emergencies	NSW Police
Fire	Fire and Rescue NSW
Hazardous Materials	Fire and Rescue NSW
Flood, storm or any natural disaster	NSW State Emergency Service

PART C – Appendices

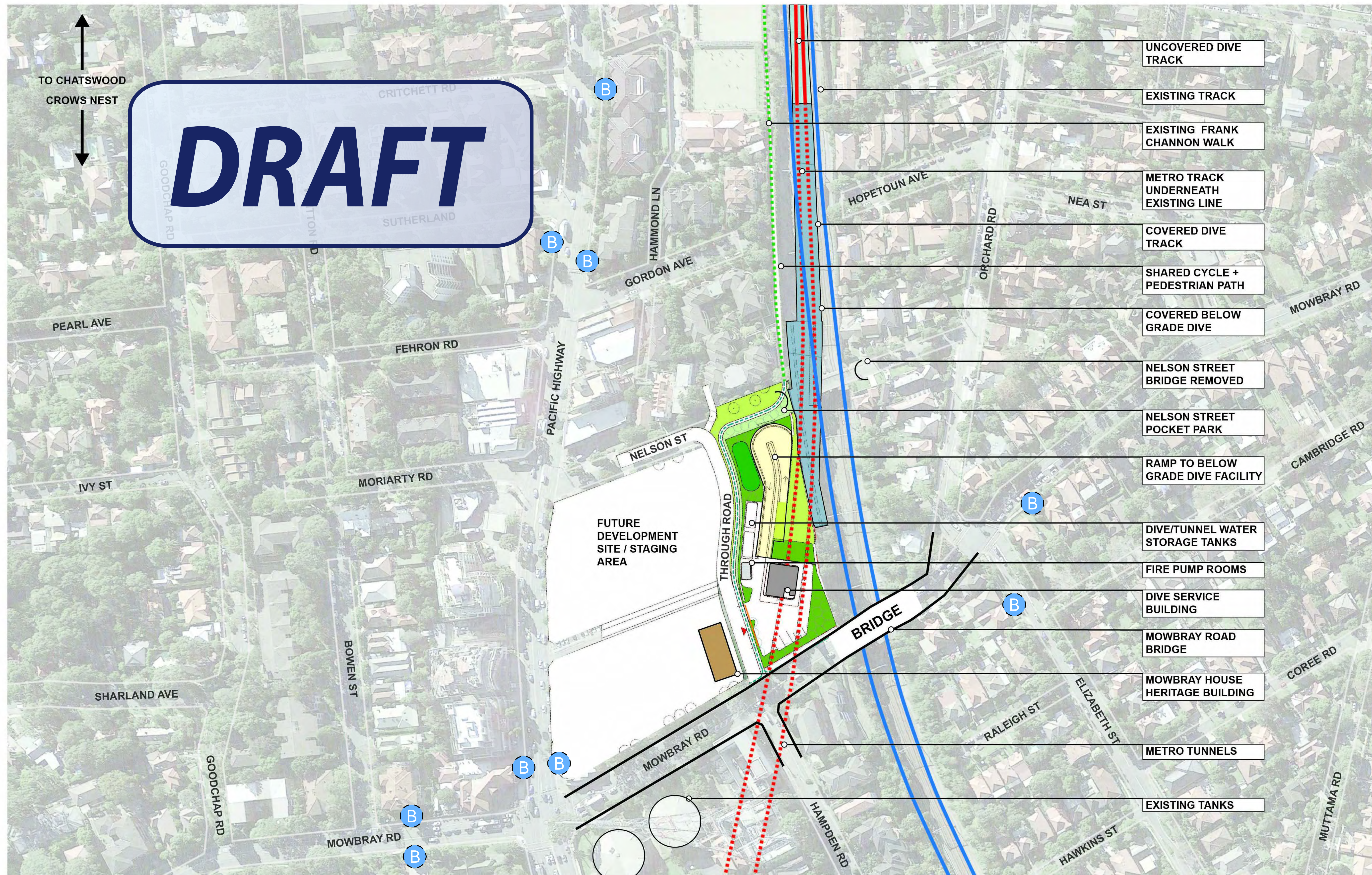
Appendix A. Concept Design Drawings (design still in progress)

(drawing removed)



TO CHATSWOOD
CROWS NEST

DRAFT



ZONING LEGEND

- RAMP DOWN TO PORTAL
- HERITAGE BUILDING
- COVERED DIVE TRACK
- UNCOVERED DIVE TRACK

- METRON TRACK LINE
- EXISTING TRACK LINE
- PORTAL BUILDING

B BUS STOP

000100 - SITE ELEMENTS

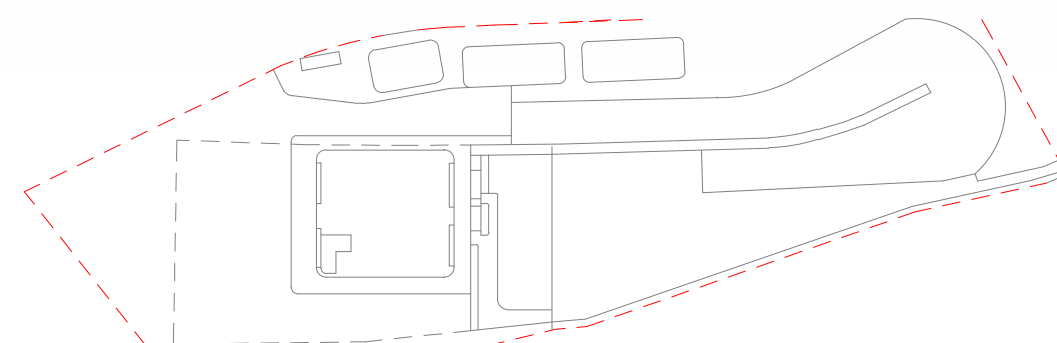
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FOR STAGE APPROVAL

REV.	BY	DATE	DESCRIPTION	APPD.
P05	KC	24/11/2017	ISSUE FOR FINAL PRELIMINARY DESIGN	MB
P04	KC	06/09/2017	ISSUED FOR CLIENT REVIEW	MB
P03	KC	21/07/2017	ISSUED FOR TECHNICAL REVIEW	MB
P02	KC	09/06/2017	ISSUED FOR INTERIM DESIGN	MB
P01	KC	19/05/2017	ISSUED BASIS OF DESIGN	MB

A1 Original Co-ordinate System: MGA Zone 56 Height Datum: A.H.D. This sheet may be prepared using colour and may be incomplete if copied



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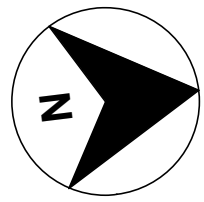
SERVICE PROVIDERS

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DESIGNED _ _ _ L. FITZGERALD _ _ _
DRG CHECK _ _ _ N. DO VALE _ _ _
DESIGN CHECK K. CARRUCAN _ _ _
APPROVED _ _ _ M. BARRON _ _ _

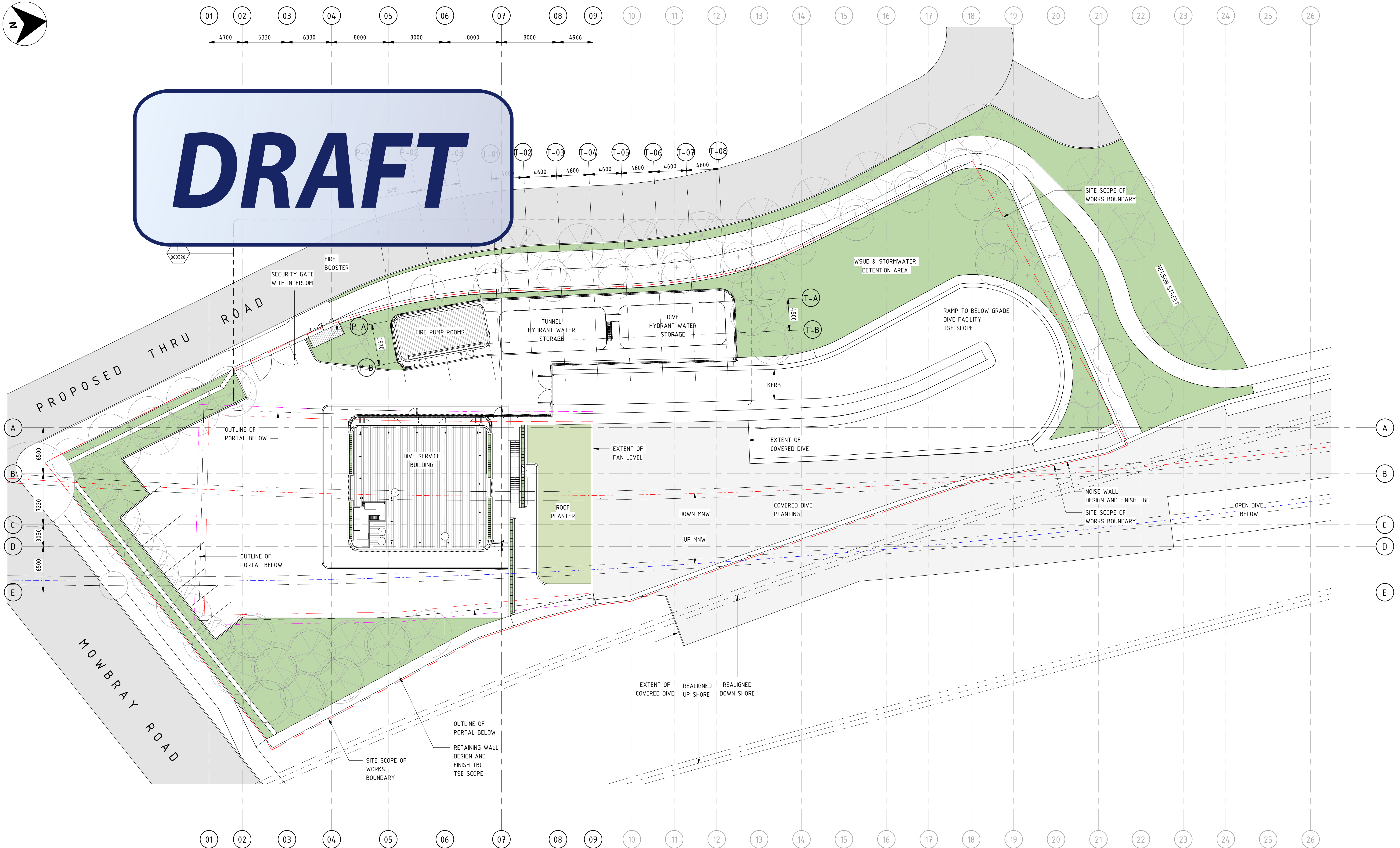


SYDNEY METRO USDTs
NORTHERN DIVE
ARCHITECTURAL DESIGN
SITE ELEMENTS

STATUS: STAGE 1C FINAL PRELIMINARY DESIGN SHEET 2 OF 5 ©
DRG No NWRLSRT-MET-SND-AT-DWG-000102 REV P05



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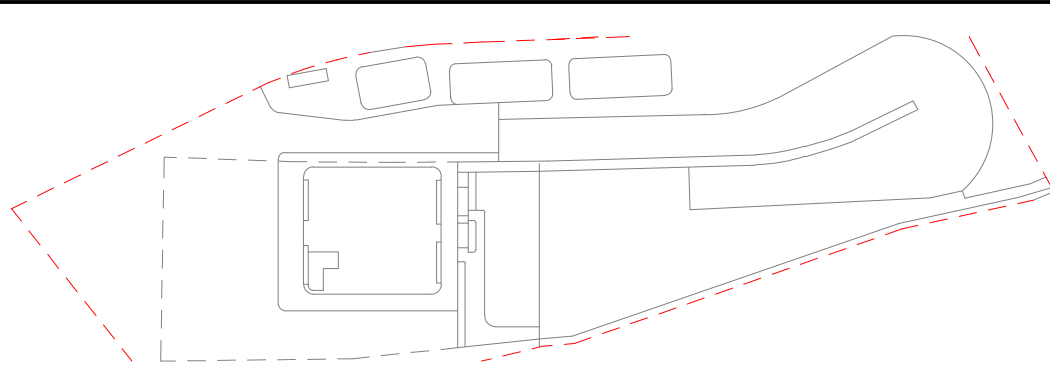
SITE PLAN

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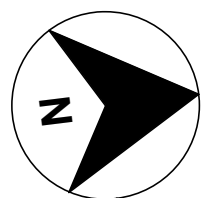
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P05	KC	06/09/2017	ISSUED FOR CLIENT REVIEW	MB
P04	KC	07/08/2017	ISSUE FOR INTERIM DISCIPLINARY CHECK	MB
P03	KC	21/07/2017	ISSUED FOR TECHNICAL REVIEW	MB
P02	KC	09/06/2017	ISSUED FOR INTERIM DESIGN	MB
P01	KC	19/05/2017	ISSUED BASIS OF DESIGN	MB
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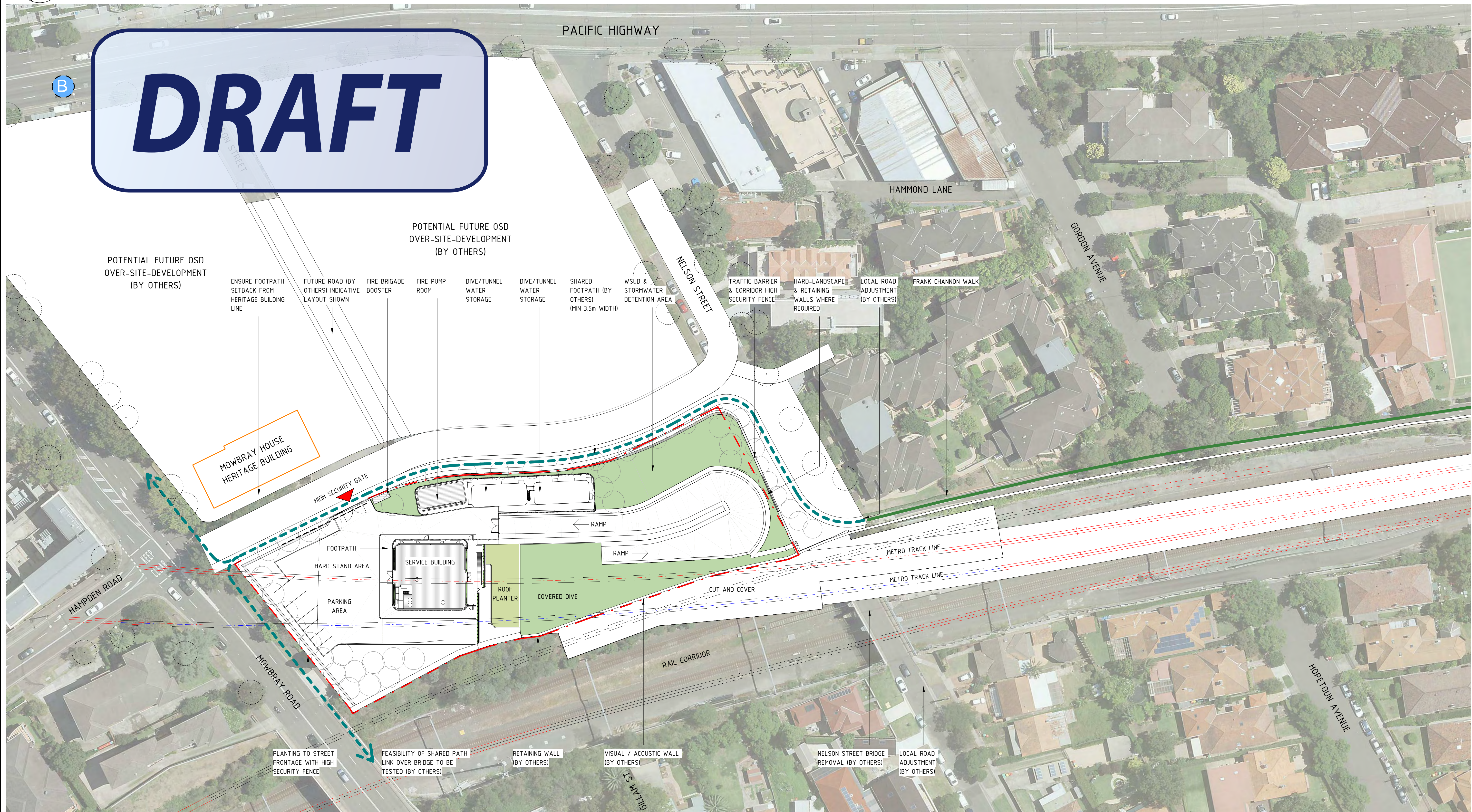


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DRG CHECK	N. DO VALE
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SITE PLAN		
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DRAFT



TREES

- EXISTING TREE (NOT SURVEYED INDICATIVE LOCATION ONLY)
- PROPOSED TREE

PLANTING

- GROUND COVER PLANTING (SHRUBS & GRASSES)
- PAVEMENT
- IN-SITU CONCRETE (PUBLIC DOMAIN AREAS ONLY)

PUBLIC DOMAIN PLAN

- SCOPE OF WORKS
- EXISTING SHARED PATH
- PROPOSED SHARED PATH LINK

- BUS STOP
- ENTRY

PUBLIC DOMAIN PLAN

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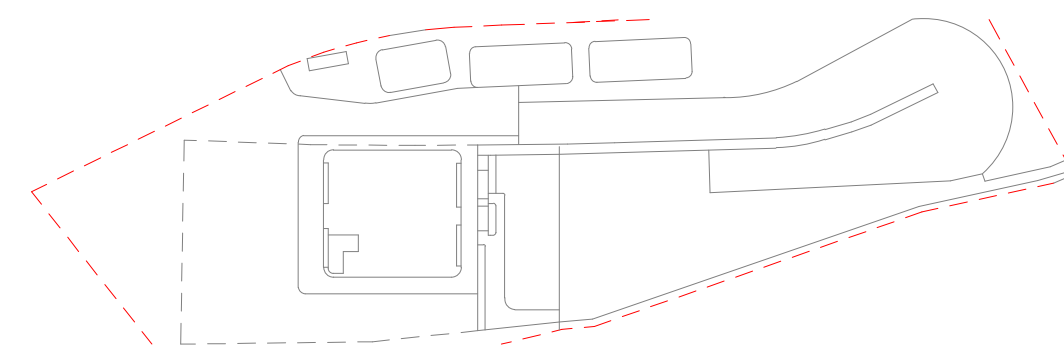
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SYDNEY METRO USDTs
NORTHERN DIVE
ARCHITECTURAL DESIGN
PUBLIC DOMAIN PLAN

STATUS: STAGE 1C FINAL PRELIMINARY DESIGN SHEET 1 OF 1
DRG No NWRLSRT-MET-SND-AT-DWG-000105 REV P05

REV.	BY	DATE	DESCRIPTION	APPD.
P05	KC	24/11/2017	ISSUE FOR FINAL PRELIMINARY DESIGN	MB
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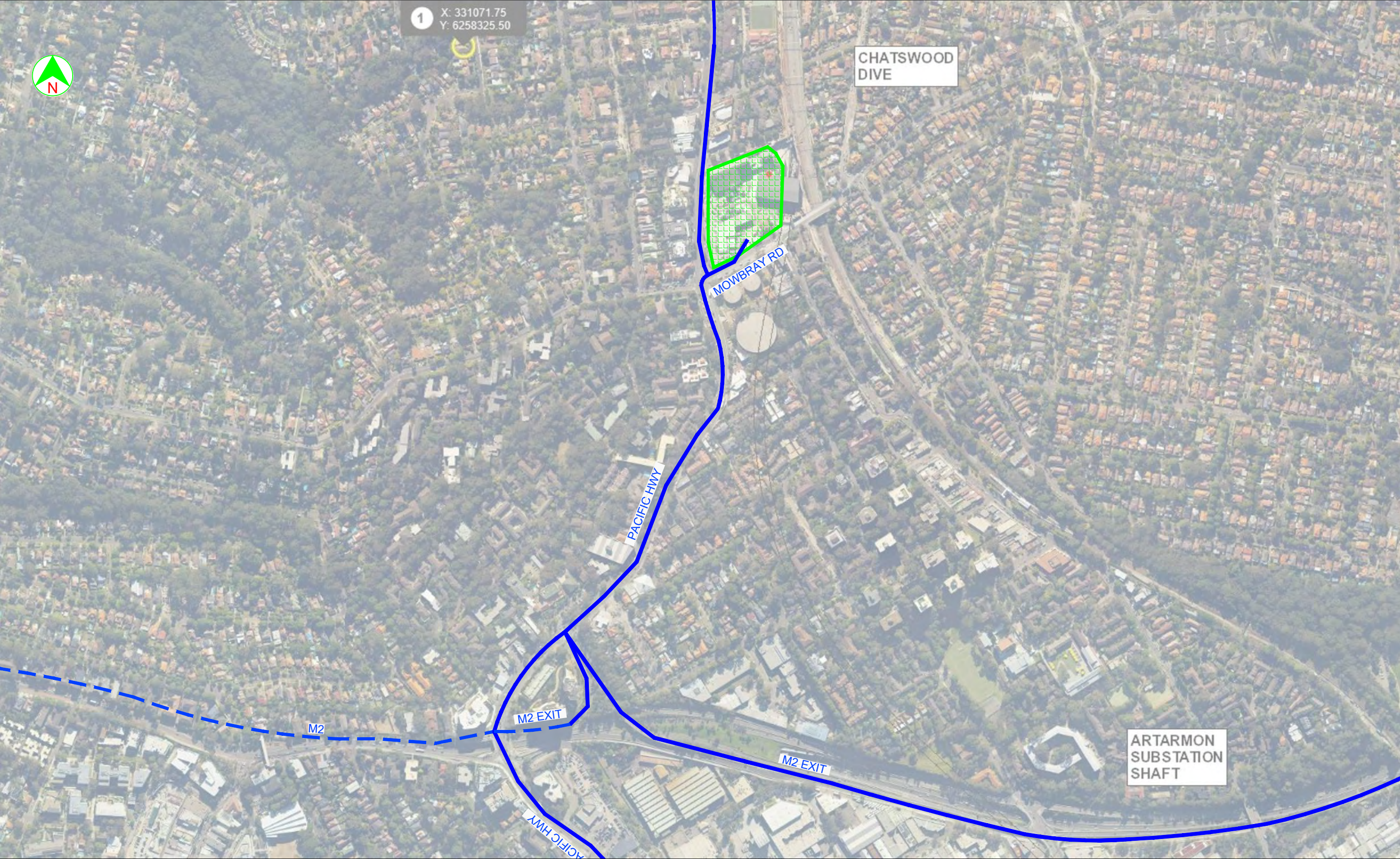
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DESIGN CHECK: K. CARRUCAN
APPROVED: M. BARRON

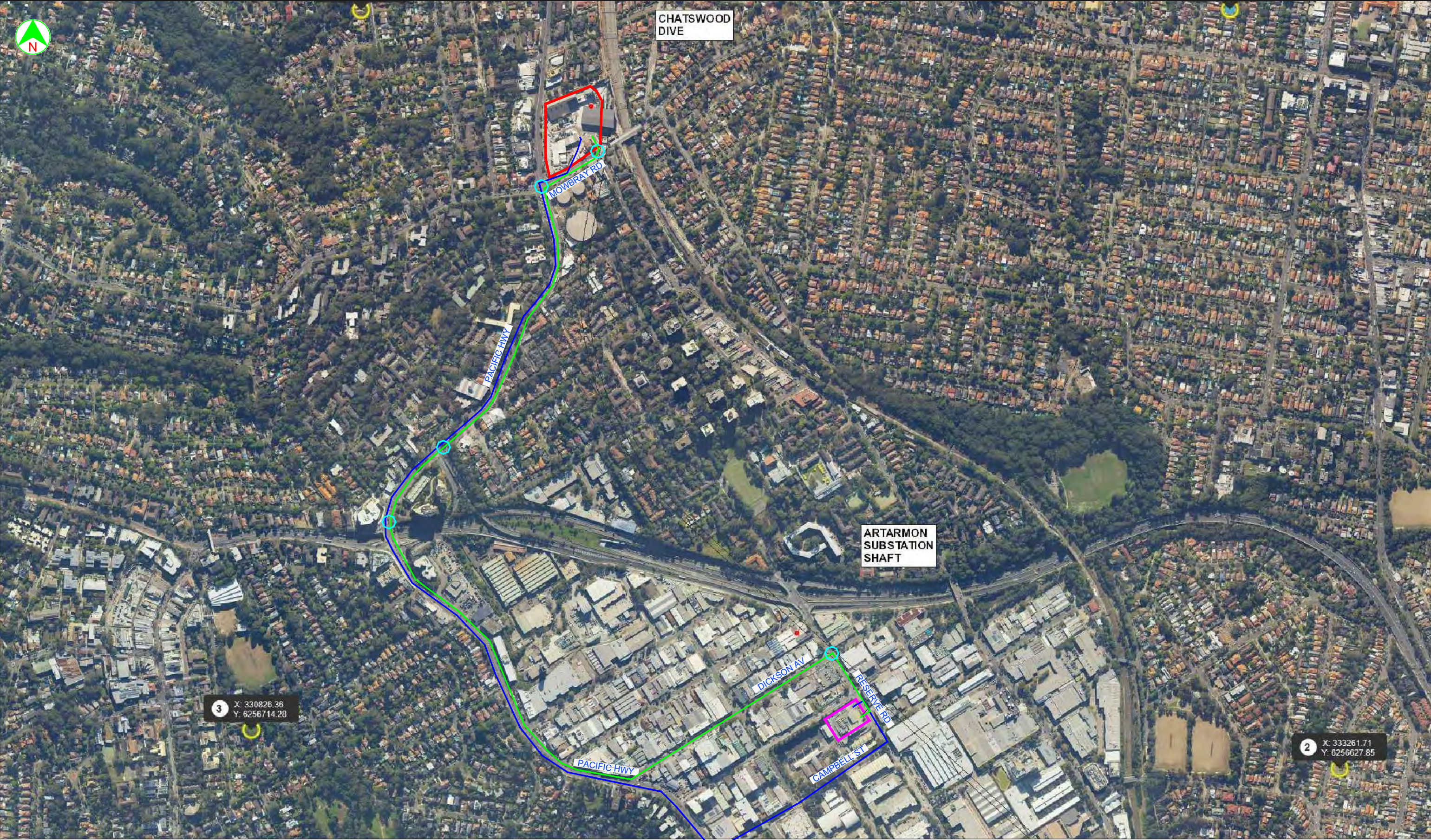
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Appendix B. Construction Vehicles Route



						NOTES	PLOT DATE / TIME			PLOT BY M SIM		CLIENT 	
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							NOTES	PLOT DATE / TIME			PLOT BY M SIM	CLIENT		Concrete delivery logistics to Northern Dive Site - from nominated concrete plant at Artarmon industrial area (other available concrete plants have similar route)			A3	
LEGEND		REVISION DESC.	REV	DATE	APPROVAL	SCALES ON A3 SIZE DRAWING		DRAWINGS / DESIGN PREPARED BY		TITLE	NAME							DATE
<div><div></div> Northern dive building site</div> <div><div></div> Nominated concrete batch plant location</div> <div><div></div> Full load run - 2.37km</div> <div><div></div> Empty load run - 2.26 km</div> <div><div></div> Signalised intersection</div>										DRAWN	M.SIM							2/3/20
										DRG CHECK	M.SIM							2/3/20
										DESIGN								
										DESIGN CHECK			PREPARED FOR					
										TRAFFIC MNGR			Systems Connect		ISSUE STATUS	SHEET No.	ISSUE	
						CO-ORDINATE SYSTEM MGA ZONE 56		HEIGHT DATUM AHD							FOR INFORMATION	1 of 1	0	

(drawing removed)

Appendix C. General correspondence etc (attach when available)

LWC General Correspondence

Reference No: SMCSWLWC-RMS-LWC-GEN-000022
Project Title: Sydney Metro City & Southwest - LWC, TSOM
Contract No: LWC - Line Wide Contracts
Sub Contract: -
Orig Ref No:
DLM:

Date: 26 May 2020, 04:19 PM **Response required by:**

From: Quac Minh LA (Roads and Maritime Services (part of TfNSW division))

To: Susan Dai (Systems Connect)

Cc: Chris Berg (Sydney Metro) ; Ken Hind (Sydney Metro) ; JOSE ARGUETADOMINGUEZ (Sydney Metro) ; Phil Brogan (Sydney Metro) ; Nathan Hoffmeister (Sydney Metro) ; Deepak Shahani (Sydney Metro) ; Errol Pather (Sydney Metro) ; Jake Coles (Sydney Coordination Office) ; Carl Mella (Roads and Maritime Services (part of TfNSW division)) ; David Tawadros (Roads and Maritime Services (part of TfNSW division)) ; Hugh Chapman (Sydney Metro) ; James Porter (Sydney Metro) ; Steve Brown (Sydney Coordination Office) ; Hayden Wright (Sydney Metro) ; Transmittal SM OpenAccess (Sydney Metro) ; Mathew Billings (Systems Connect) ; Mark Marriott (Sydney Metro) ; Jill Downing (Systems Connect) ; Kirimaru Friscan (Systems Connect) ; LWC Systems Connect Transfer (Systems Connect) ; Paul Ryan (Systems Connect) ; Helena Orel (Systems Connect) ; Mathew Johnston (Systems Connect) ; Mong Sim (Systems Connect)

Subject: **Construction Traffic Management Plan - Northern Dive Site Operation - TfNSW (former RMS) comments**

Hi Susan,

In response to your transmittal SMCSWLWC-SYC-TX-002461 dated 14/05/20.

TfNSW (former RMS) has no further comments and is satisfied this can be submitted for approval, pending confirmation that comments from all other stakeholders are satisfactorily addressed.

regards,
Minh

	Design Series:	
Discipline:	Design Lots:	Location:

LWC General Correspondence

Reference No: SMCSWLWC-RMS-LWC-GEN-000024
Project Title: Sydney Metro City & Southwest - LWC, TSOM
Contract No: LWC - Line Wide Contracts
Sub Contract: -
Orig Ref No: SMCSWLWC-SYC-TX-002677
DLM:

Date: 10 June 2020, 10:19 AM **Response required by:**

From: Carl Mella (Roads and Maritime Services (part of TfNSW division))

To: Susan Dai (Systems Connect)

Cc: Chris Berg (Sydney Metro) ; Ken Hind (Sydney Metro) ; JOSE ARGUETADOMINGUEZ (Sydney Metro) ; Phil Brogan (Sydney Metro) ; Nathan Hoffmeister (Sydney Metro) ; Deepak Shahani (Sydney Metro) ; Errol Pather (Sydney Metro) ; Quac Minh LA (Roads and Maritime Services (part of TfNSW division)) ; Jake Coles (Sydney Coordination Office) ; David Tawadros (Roads and Maritime Services (part of TfNSW division)) ; Hugh Chapman (Sydney Metro) ; Ali Faniad (Sydney Metro) ; Oscar Wang (Sydney Metro) ; Hayden Wright (Sydney Metro) ; Transmittal SM OpenAccess (Sydney Metro) ; Mathew Billings (Systems Connect) ; Mark Marriott (Sydney Metro) ; Jill Downing (Systems Connect) ; Kirimaru Friscan (Systems Connect) ; LWC Systems Connect Transfer (Systems Connect) ; Paul Ryan (Systems Connect) ; Helena Orel (Systems Connect) ; Mathew Johnston (Systems Connect) ; Mong Sim (Systems Connect) ; John Grant (Systems Connect)

Subject: **Construction Traffic Management Plan - Northern Dive Site Operation - TfNSW (former RMS) Approval**

Susan,

In accordance with Schedule C1 Appendix A.9 Section 2.1 (c) and 2.2 (c) of the Principal's General Specifications G10 – Traffic and Transport Management and Minister's Condition of Approval E82 for the Sydney Metro City & South West, Transport for NSW - Greater Sydney - Planning and Programs, and the Sydney Coordination Office approve the Sydney Metro City & South West Construction Traffic Management Plan – Line Wide Works Northern Dive Site Operations - SMCSWLWC-SYC-DCH-TF-PLN-002505.00.INF.00.01 subject to the following requirements:

- obtaining Road Occupancy Licenses (RoLs) from the Transport Management Centre as required;
- complying with construction vehicle routes as approved by SCO/TfNSW and shown in Appendix B of the CTMP;
- addressing any safety issues identified within the Road Safety Audit review for this CTMP, in advance of any works commencing;
- addressing any issues raised by Council, STA, Taxi Council, residents/businesses or Emergency Services in the CTMP approval process;
- addressing the requirements arising as an outcome of the Local Traffic Committee meeting;
- promptly addressing any SCO and/or TMC and/or TfNSW issue that eventuates during the works

Regards,
Carl

Sim, Mong

From: Philip Brogan <Philip.Brogan@transport.nsw.gov.au>
Sent: Friday, 12 June 2020 10:41 AM
To: Sim, Mong
Cc: Nathan Hoffmeister; Ben Hodgson-armstrong; Kenneth Hind
Subject: Linewide CTMP
Attachments: SMCSWLWC-SYC-DCH-TF-PLN-002505.00.INF.00.01.pdf; Linewide CTMP Chatswood Approval Notification 10 June 2020.pdf

Importance: High

CAUTION: This email originated from outside of the Organisation.

Mong,

Attached is the approved Linewide CTMP for the Northern Dive site Operations and a PDF copy of the 10 June 2020 CTMP approval. The document requires amendment and resubmission to Transport Coordination for endorsement and Planning and Programs for approval. The document, in its current form does not satisfy the Metro planning approval requirements. The document (Section 1.3) refers to the incorrect planning approval for this site. Additionally:

1. The overall quality of the document requires improvement.
2. Section 1.3, second bullet point, needs to clarify which CSSI or REMMs are being referred to.
3. Section 2.1, this section reads like the site will be used as an Ancillary Facility, but it is not clear how this aligns with A16-17.
4. Figure 7 - description states picture is looking north from Pacific Highway, while in fact it is looking south.
5. Part C, Appendix A - the concept design drawing refers to a 'Through Road' between Nelson St and Mowbray Rd which has to my knowledge not been assessed or approved.
6. Part C, Appendix A - the second concept design drawing refers to a 'Thru Road' which requires clarification.

Before I advise Transport Coordination and Planning and Programs of the need to amend and resubmit, can you please confirm that this does not create works issues for the Linewide contractor.

Regards

Philip Brogan
Consultant
Operations, Customer & Placemaking
Sydney Metro

T 0401 719 632
Level 40, 680 George Street, Sydney 2000



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Consider the environment. Please don't print this e-mail unless really necessary.

Design Series:		
Discipline:	Design Lots:	Location:

Appendix D. Compliance Matrix Checklist

Relevant conditions of approval.

Line	Conditions	SC Notes	Ref.	Compliance
E75	The CSSI must be designed, constructed and operated with the objective of integrating with existing and proposed road and related transport networks and minimising adverse changes to the safety, efficiency and, accessibility of the networks, and facilitate an improved level of service in relation to permanent and operational changes. Detailed design and assessment of related traffic, parking, pedestrian and cycle accessibility impacts and changes shall be undertaken:	Northern Dive buildings work is utilising an existing site establishment.	2.1	Y
E75(a)	in consultation with, and to the reasonable requirements of the Traffic and Transport Liaison Group(s) established under Condition E77;	Refer to E81.	5	Y
E75(b)	in consideration of existing and future demand, connectivity (in relation to permanent changes), performance and safety requirements;	The work site does not involve any changes to existing road conditions	n/a	n/a
E75(c)	to minimise and manage local area traffic impacts;	Current access to Northern dive building site is not through local roads.	n/a	n/a
E75(d)	to ensure access is maintained to property and infrastructure; and	Current access to Northern dive building site is not impacting any properties	n/a	n/a
E75(e)	to meet relevant design, engineering and safety guidelines, including Austroads, Australian Standards, and RMS (RTA) requirements.	CTMP development is in reference to these documents. E75(e) serves as a reminder to ensure latest requirements and standards are considered in the CTMP development.	1.4	Y
E75	Copies of civil, structural and traffic signal design plans shall be submitted to the Relevant Road Authority for consultation before the commencement of the relevant works.	No new proposed signal on this CTMP.	n/a	n/a
E76	Permanent road works, including vehicular access, signalised intersection works, and works relating to pedestrians, cyclists, and public transport users must be subject to safety audits demonstrating consistency with relevant design, engineering and safety standards and guidelines. Safety audits must be prepared in consultation with the Traffic and Transport Liaison Group before the completion and use of the subject infrastructure and must be made available to the Secretary upon request.	E76 refers to permanent work. Ausroads' Guide to Road Safety Part 6: Road Safety Audit has published "...Great effort has been taken by road authorities to provide adequate safety by the development of worksite safety manuals and roadworks code of practice. It may be considered that these practices provide sufficient safety without the need for audits of temporary traffic arrangement." RSA on a temporary traffic management may not be providing any beneficial value. The site operations does not change from its current operations.	1.2	Y
E79	The Proponent must consult with the Relevant Road Authority regarding the use of any weight restricted road by heavy vehicles.	Routes proposed not travelling through weight restricted roads.	n/a	n/a
E80	The Proponent must minimise truck movements during peak periods within commercial centres. Peak periods are 7am to 10am and 4pm to 7pm Monday to Friday.	Proposed trucks movement to this site is not within commercial centres.	n/a	n/a
E81	The Proponent must prepare and implement a Construction Traffic Management Framework (CTMF). The CTMF must be prepared in consultation with TTLG(s) and submitted to the Secretary for approval no later than one (1) month before the commencement of construction (or within any other timeframe agreed with the Secretary). The CTMF will set out the approach to managing issues across the CSSI and include but not be limited to:	Refer to E82.	1.3	Y
E81(a)	construction site access, including the efficient and safe egress and ingress of vehicles, consistent relevant Austroads, Australian Standards and RMS requirements;	Entry and exit to site are existing conditions.	n/a	n/a
E81(b)	the erection and maintenance of hoardings, scaffolds and associated structures on roads;	No such structures are proposed.	n/a	n/a
E81(c)	short and long term lane and road closures including those associated with plant, crane and other operations between the road reservation and construction site;	No lane closure are proposed.	n/a	n/a
E81(d)	cumulative construction vehicle management from surrounding developments;	TSE work is completed before handing over to LW. No other known development at this stage.	n/a	n/a
E81(e)	bus stop and associated facilities relocation and service rerouting;	No bus stop along Pacific Highway needs to be relocated or impacted.	n/a	n/a
E81(f)	short and long term works zones on roads adjacent to the construction site;	No work zone applied adjacent to construction site.	n/a	n/a
E81(g)	mail zone and associated facilities relocation;	No proposed relocation of mail zone.	n/a	n/a
E81(h)	short and long term works within the road reservation;	No road upgrade within LW scope for Northern Corridor upgrade.	n/a	n/a
E81(i)	regulatory, advisory and other signage changes and modifications;	No new proposed signages.	n/a	n/a
E81(j)	parking management, including on and off street and remote parking and access;	Parking is available within the compound in general. Refer condition in T12.	3.1	Y
E81(k)	heavy vehicle management, the restriction (unless otherwise approved) of heavy vehicles to certain routes and the minimisation of heavy vehicle traffic in peak traffic periods;	(same as E80). Trucks are programmed to take advantage of lesser traffic volume at night hours.	3.6	Y
E81(l)	special event management;	n/a along Pacific Hwy or Mowbray Road.	n/a	n/a
E81(m)	the retention and reinstatement of emergency and property access;	Access is maintained. No modification to any access or driveways.	3.3	Y
E81(n)	the retention of user and passenger safety, including pedestrians, cyclists, public transport users, including at stops and related facilities;	No road is closed.	3.2	Y
E81(o)	incident response planning around construction worksites; and	Item identified on Section 1.2 bullet point 8.	1.2; 7	Y
E81(p)	monitoring of transport and access related impacts attributable to the CSSI.	Item identified on Section 1.2 bullet point 9. Refer to E75 (b) and (c).	1.2	Y
E82	Construction Traffic Management Plans (CTMPs), consistent with the CEMF and CTMF required in Condition E81, must be prepared for each construction site in consultation with the TTLG(s), and submitted to the RMS for approval following Sydney Coordination Office endorsement before construction commences at the relevant construction site. A copy of any Construction Traffic Management Plans approved by the RMS must be submitted to the Secretary for information.	CTMP prepared with CTMF guidelines. CTMP first presented on TCG on 14 Jan 2020 and TTLG on 29 Jan 2020.	5	Y
E83	Where construction results in a worsening of the matters identified in Condition E81(a)-(o), the Proponent must review the measures identified in the CTMPs in consultation with the TTLG(s), as relevant. Any changes to the CTMPs must be submitted to the RMS for approval following Sydney Coordination Office endorsement and implemented.	This item was identified on Section 1.2 bullet point 9. CTMP review and update as required.	1.2	Y
E85	Heavy vehicle haulage must not use local roads unless no feasible alternatives are available.	Access to Northern dive site is not via local roads.	n/a	n/a
E86	During construction, measures must be implemented to maintain pedestrian and vehicular access to, and parking in the vicinity of, businesses and affected properties. Alternative pedestrian and vehicular access, and parking arrangements must be developed in consultation with affected businesses. Such arrangements must be outlined in the Business Management Plan required in Condition E64 and implemented as required. Adequate signage and directions to businesses must be provided before, and for the duration of, any disruption.	The CTMP does not propose any road closures etc.	n/a	n/a
E88	Details of haulage routes and heavy vehicle sizes to transport material to and from any construction site must be specified in the Construction Traffic Management Plan(s) and be approved by the RMS following endorsement by Sydney Coordination Office and consultation with the TTLG(s).	Transportation details available on Section 3.6 in the CTMP with relevant information regarding proposed vehicle routes and estimated vehicles movements.	3.6; App B.	Y
E89	The Proponent must implement traffic and transport management measures with the aid of a truck marshalling and logistics facility located within close proximity to the Sydney and North Sydney CBDs. The facility must be operational in advance of tunnel spoil generation. Details of the facility must be documented in the Ancillary Facilities Management Plan required by Condition A16.	(not applicable for this site)	n/a	n/a
E90	A Road Dilapidation Report must be prepared for local roads proposed to be used by heavy vehicles for the purposes of the CSSI before the commencement of use by such vehicles. Copies of the Road Dilapidation Report must be provided to the Relevant Council within three (3) weeks of completing the surveys and no later than one (1) month before the use of local roads by heavy vehicles.	Not applicable. Routes to northern dive building site does not utilise local roads.	n/a	n/a
E91	If damage to roads occurs as a result of construction of CSSI, the Proponent must either (at the landowner's discretion):	E91 is subject to E90.	n/a	n/a
E91(a)	compensate the landowner for the damage so caused. The amount of compensation may be agreed with the landowner; or	E91(a) is subject to E90.	n/a	n/a
E91(b)	rectify the damage so as to restore the road to at least the condition it was before construction commenced as identified in the Road Dilapidation Report(s).	E91(b) is subject to E90.	n/a	n/a
T1	Ongoing consultation would be carried out with (as relevant to the location) the CBD Coordination Office, Roads and Maritime Services, Sydney Trains, NSW Trains, the Port Authority of NSW, Barangaroo Delivery Authority, local councils, emergency services and bus operators in order to minimise traffic and transport impacts during construction.	In addition to CTMP Section 1.2, an active communications with the group is in place.		Y
T2	Road Safety Audits would be carried out at each construction site. Audits would address vehicular access and egress, and pedestrian, cyclist and public transport safety.	Refer to E76.	1.2	Y

T3	Directional signage and line marking would be used to direct and guide drivers and pedestrians past construction sites and on the surrounding network. This would be supplemented by Variable Message Signs to advise drivers of potential delays, traffic diversions, speed restrictions, or alternate routes.	Not applicable as no pavement realignment is to be completed.	1.2	Y
T4	In the event of a traffic related incident, co-ordination would be carried out with the CBD Coordination Office and / or the Transport Management Centre's Operations Manager.	Report traffic related incident to TMC command centre not to "CBD Coordination Office".	7	Y
T5	The community would be notified in advance of proposed road and pedestrian network changes through media channels and other appropriate forms of community liaison.	Community notification is in the early progress.	5	Y
T6	Vehicle access to and from construction sites would be managed to ensure pedestrian, cyclist and motorist safety. Depending on the location, this may require manual supervision, physical barriers, temporary traffic signals and modifications to existing signals or, on occasions, police presence.	Existing temporary signal is in operations.	3.3	Y
T8	Access to existing properties and buildings would be maintained in consultation with property owners.	No properties are impacted from this site operations.	3.3	Y
T9	All trucks would enter and exit construction sites in a forward gear, where feasible and reasonable.	Entry and exit gate available.		Y
T12	Construction sites would be managed to minimise construction staff parking on surrounding streets. The following measures would be implemented: »Encouraging staff to use public or active transport »Encouraging ride sharing »Provision of alternative parking locations and shuttle bus transfers where feasible and reasonable. Transport for NSW would work with local councils to minimise adverse impacts of construction on parking and other kerbside use in local streets, such as loading zones, bus zones, taxi zones and coach zones.	The northern dive site has some parking capacity. The close proximity of Chatwood and Artarmon train stations automatically promote the use of public transportation and in effect eliminates off street parking.	App B.	Y
T13	Construction site traffic would be managed to minimise movements in the AM and PM peak periods.	Trucks are adjusted to take advantage of the lesser traffic conditions from 7pm to 5am time where trucks are generating better average speed, better fuel management and have less idle and stationary time as shown on the table.	3.6	Y
T14	Construction site traffic immediately around construction sites would be managed to minimise movements through school zones during pick up and drop off times.	No schools immediately within the work area.	n/a	n/a
T19	Where existing parking is removed to facilitate construction activities, alternative parking facilities would be provided where feasible and reasonable.	No modifications to existing conditions.	n/a	n/a
T21	The potential combined impact of trucks from multiple construction sites would be further considered during the development of Construction Traffic Management Plans.	TSE work is to be completed before mobilisation. If there is a potential of overlap between groups during the transition (handing over), the TSE is at the end state of the their work peak and does not generate trucks volume that is significant.	5	Y
T22	Where existing footpath routes used by pedestrians and / or cyclists are affected by construction, a condition survey would be carried out to confirm they are suitable for use (eg suitably paved and lit), with any necessary modifications to be carried out in consultation with the relevant local council.	Not applicable for this site	n/a	n/a

Appendix E. Traffic Management Plan – Mowbray Road Utilities Work

1.1. Introduction

The Northern Connection site is near completion and utilities commissioning and connectivity phase is soon underway. The new connection of watermain and sewerage services to the site requires tapping into the source of supply (water and sewer runoffs) from the existing underground services network located under Mowbray Road eastbound carriageway. The work will need traffic management to channel through traffic away from the excavation pit. The work should take approximately 14 night shifts to complete. Road plates will be used to temporarily cover the opening for the subsequent night shifts.

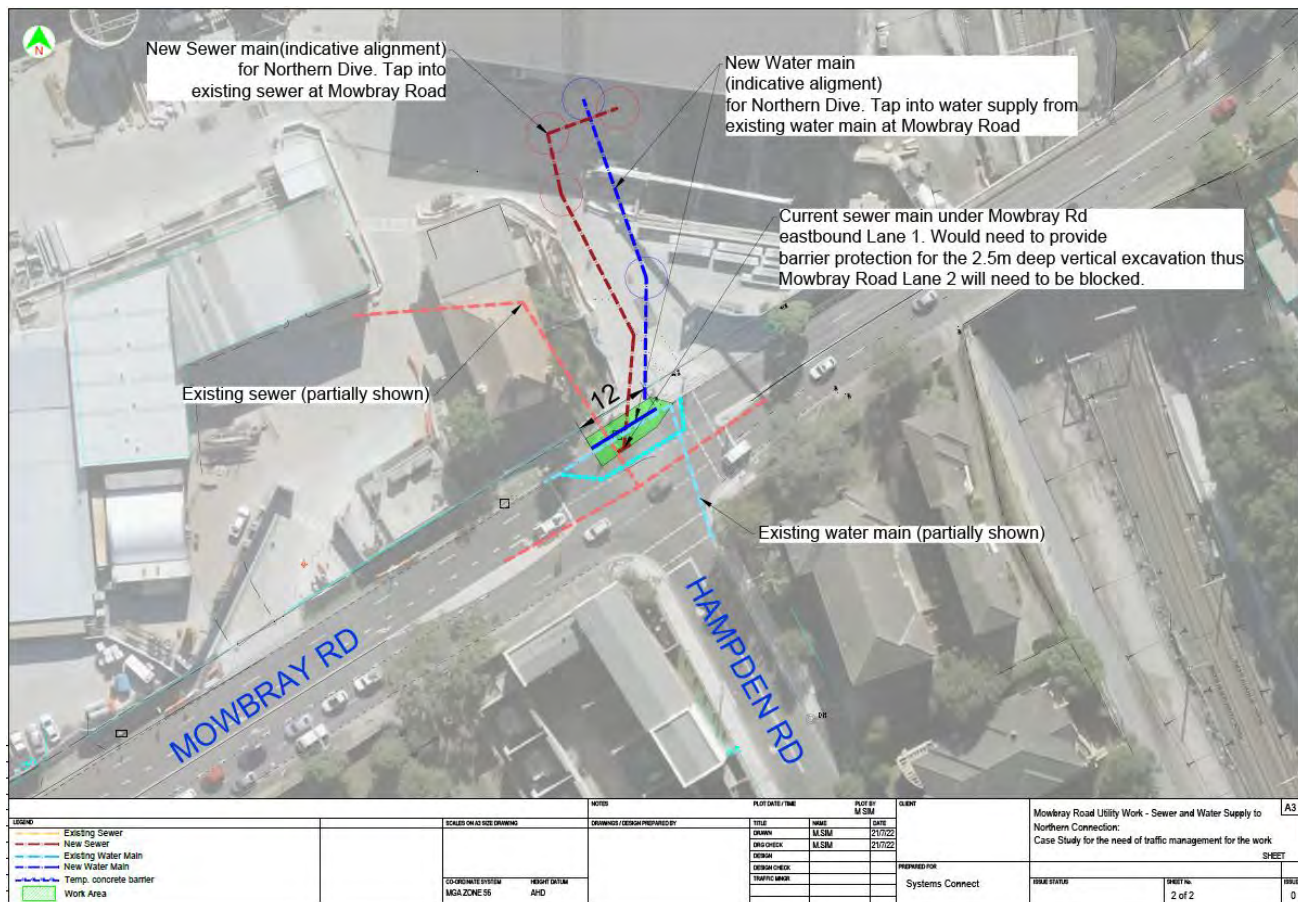
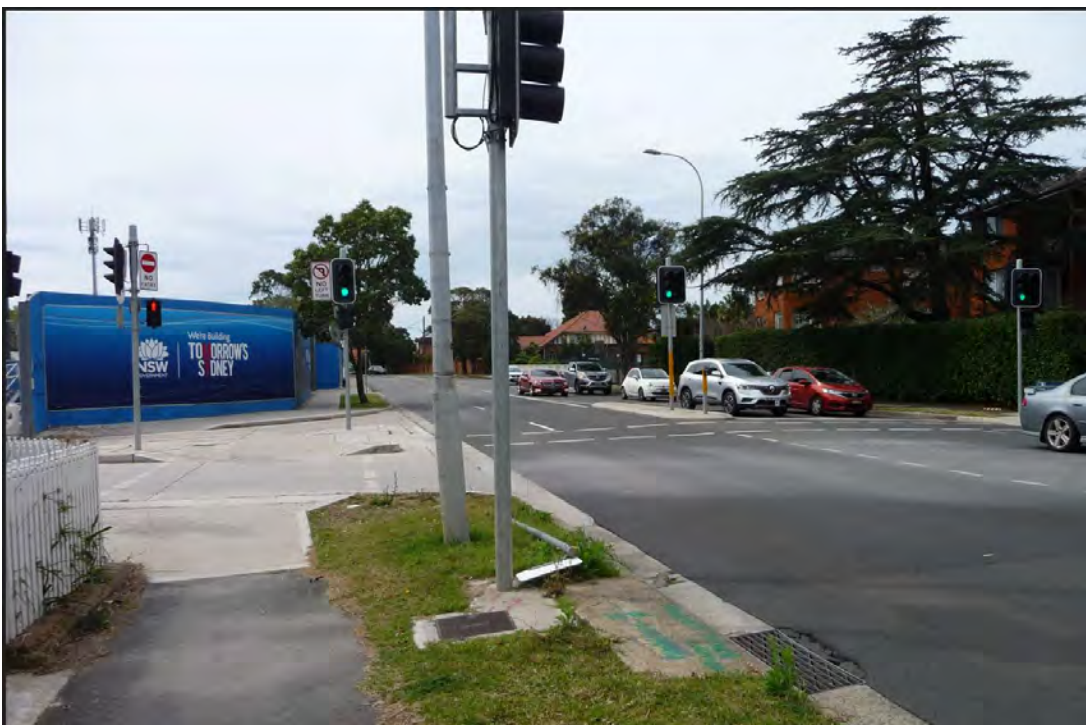


Figure 1. Locality of the work area at Mowbray Road, Chatswood

To reach the location of the sewer and watermain at approximately 2.5m under Mowbray Road eastbound road, there are three (3) traffic management options that were considered for enabling the work. All the options have its advantages and disadvantages. Due to the depth of the excavation, Lane 2 Mowbray Road eastbound is recommended to be closed. Refer to Section 1.3.

An early work to investigate and verify the underground utilities will need to be undertaken prior to actual excavation. Refer to Section 1.2.



1.2. Traffic Management for early work survey

A combination of TGS for a standard lane closure and/or a mobile work may be applied to enable survey of the underground utilities prior to the actual excavation work. The surveying work may best be completed under a mobile work due to the survey work itself is highly mobile and short occupancy of any point/grid of the area surveyed.

1.3. Traffic Management for actual excavation

The 3 traffic management options for the excavation work are summarised below:

Options	Pros	Cons	Notes / Conclusion
Option A – detour.	<ul style="list-style-type: none"> Maximum working area and total separation from external traffic. Traffic is “free flowing” despite the <i>detour</i> as no traffic are held up at the slow point. 	<ul style="list-style-type: none"> Inconvenience to general westbound traffic as the detour is not straight forward especially for traffic coming from Elizabeth St. Approval for detour could take many weeks Detour route has the potential to conflict with the other work area (Brand St drainage detour) depending on timing 	<ul style="list-style-type: none"> Best for constructibility but weighting on community impact, scope of work etc it does not appear to warrant the necessity of a detour. It will be the least preferred option.
Option B – contraflow.	<ul style="list-style-type: none"> Through traffic for all directions for Mowbray Road are maintained albeit reduced to one lane each directions 	<ul style="list-style-type: none"> Mowbray Road westbound where the contraflow is 6.5m wide providing 3.25m each lanes. Access into property # 342 and # 346 will be not be possible due eastbound traffic is being merged to the left thus not possible to make the right turn into the property. Right turn traffic into Hampden Road will require to take a short detour to avoid interruption to Mowbray Road on both directions 	<ul style="list-style-type: none"> Second preferred option. TCS needs to be switched to flashing whilst this traffic management setup is in place. Westbound Mowbray Road needs to be merged to one lane. At the proposed time of work – night time, Hampden St movement would be low and should be manageable
Option C – crossover into intended lane.	<ul style="list-style-type: none"> Through traffic for all directions are maintained with westbound traffic returns to intended side of the road 	<ul style="list-style-type: none"> This option depends on the actual excavation width on site. If the opening is towards the east, the crossover back to the eastbound carriageway maybe too sharp. Access into property # 342 and # 346 will be not be possible due to the quick subsequent change of direction in a very short span Right turn traffic into Hampden Road will require to take a short detour to avoid interruption to Mowbray Road on both directions 	<ul style="list-style-type: none"> First preferred option provided the excavation area could be shortened. TCS needs to be switched to flashing whilst this traffic management setup is in place. Westbound Mowbray Road still needs to be merged to one lane. At the proposed time of work, Hampden St movement would be low and should be manageable

Option C is the preferred option.

1.4. Traffic Impact Summary

Traffic management will start from the hour where bulk of traffic volume begin to ease from typically 10pm (or earlier on somedays eg Sunday) and work will be demobilised before traffic volume begins to saturate from 5am or later. Completing the work within this time frame reduce significantly any risk of causing major traffic impact.

1.4.1 Buses

There are no known bus routes along Mowbray Road between Pacific Highway and Stanley Street within the work area.

1.4.2 Residents

Any entry attempt to enter property # 342 and 344 driveway from Mowbray Road eastbound may cause queuing and potential rear end collision if anyone attempt to turn right into the driveway coming from either the contraflow or the crossover options. To eliminate this risk, access into these 2 address from Mowbray Road eastbound will be temporarily disallowed during the work. All residents will be informed of this work and the no

1.4.3 Pedestrians

1.4.4 Emergency Services

1.5. Work Program

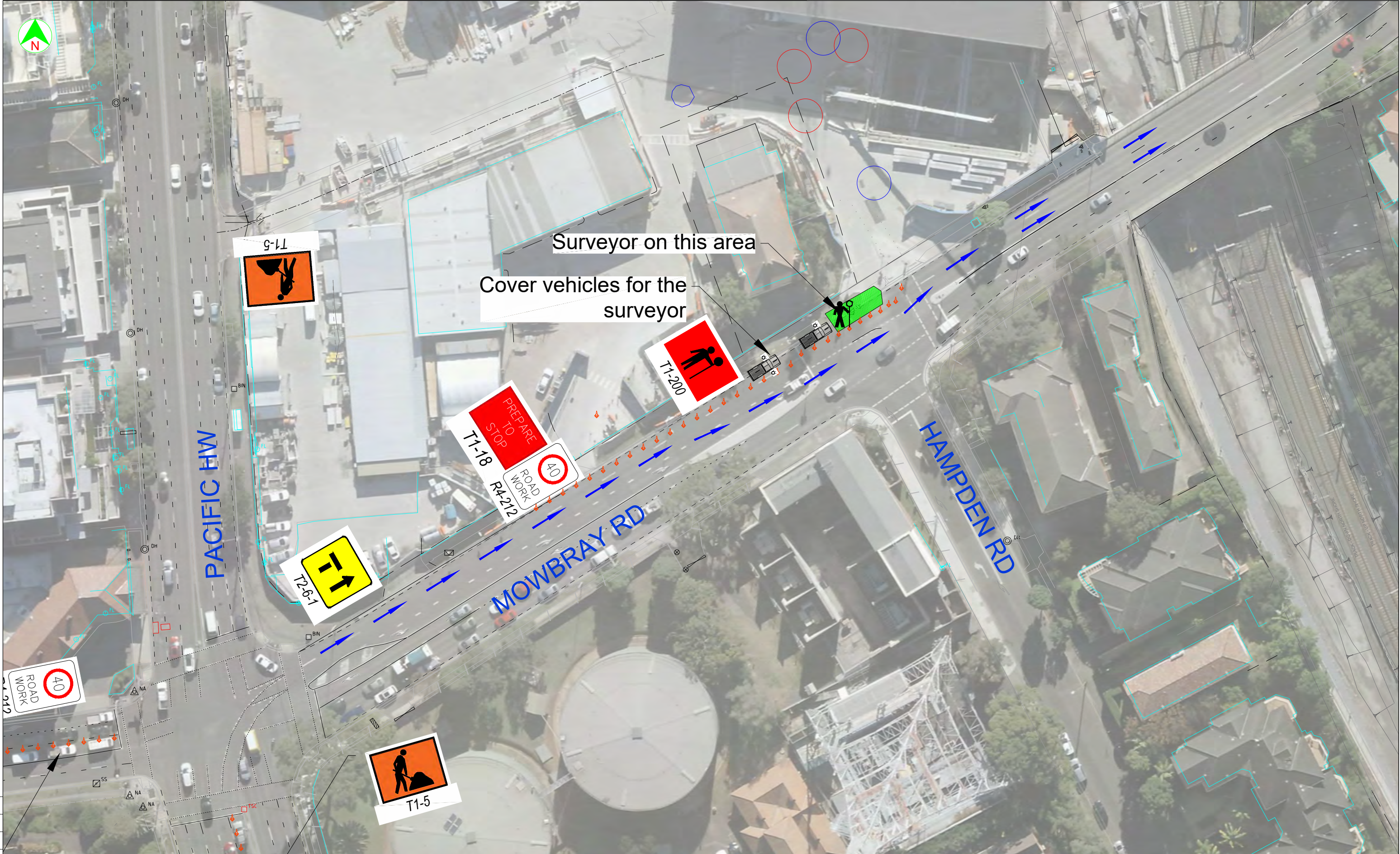
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1.6. Attachments – TGSs, design drawings

1.6.1 Survey Work TCPs (Lane 1 closure and Lane 2 mobile work)






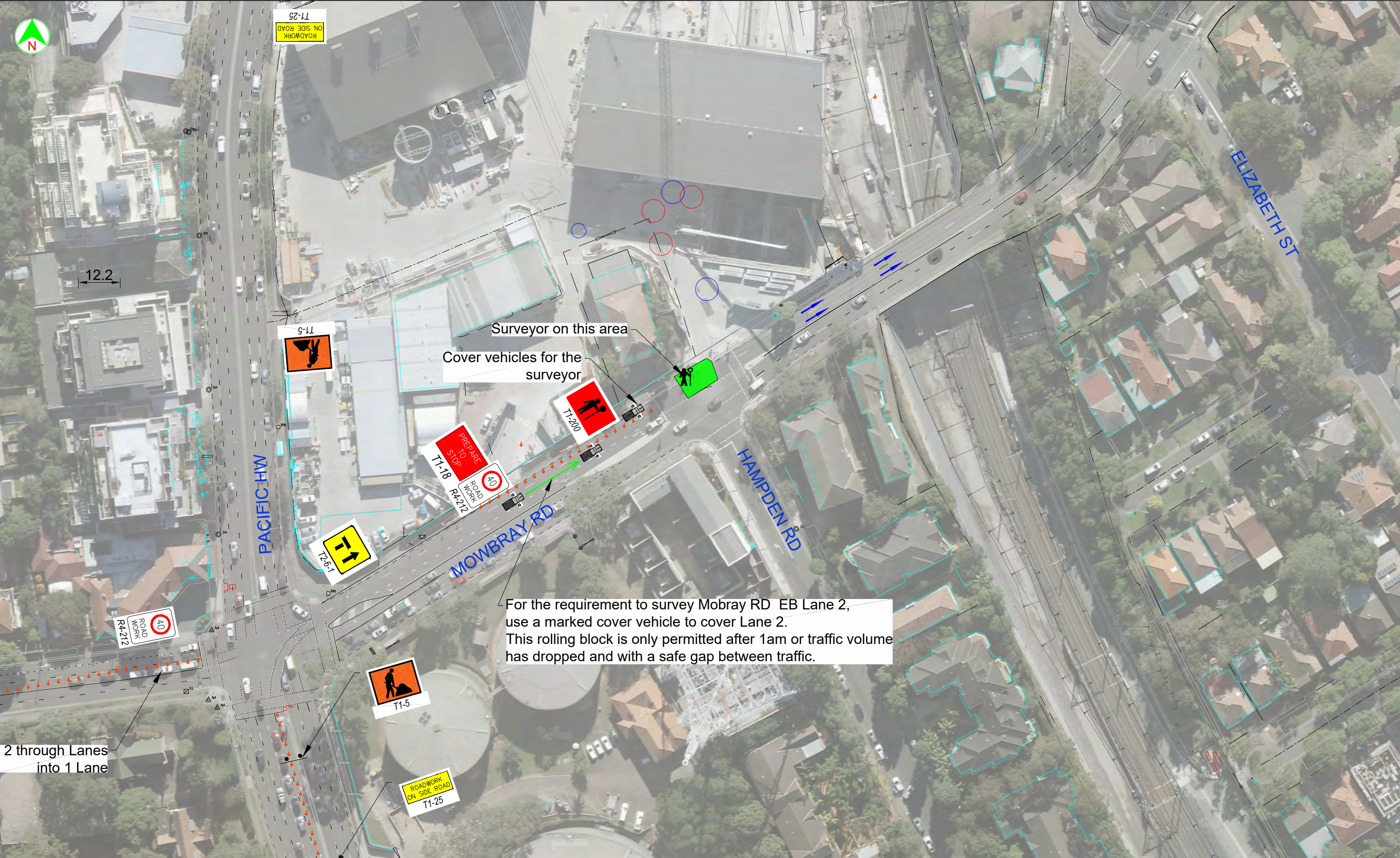
LEGEND			NOTES		PLOT DATE / TIME		PLOT BY		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.		A3
Traffic direction			DRAWINGS / DESIGN PREPARED BY		TITLE		NAME		DATE		Mowbray Road Survey Work Lane 1 closure		SHEET
Detour for Hampden Rd southbound traffic ONLY					DRAWN		M.SIM		21/7/22				
Temporary concrete barrier					DRG CHECK		M.SIM		21/7/22				
Work Area					DESIGN								
					DESIGN CHECK								
					TRAFFIC MNGR								



LEGEND		NOTES		PLOT DATE / TIME		PLOT BY		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.		A3
Traffic direction		DRAWINGS / DESIGN PREPARED BY		TITLE		NAME		DATE		Mowbray Road Survey Work Lane 1 closure		SHEET
Detour for Hampden Rd southbound traffic ONLY				DRAWN		M.SIM		21/7/22				
Temporary concrete barrier				DRG CHECK		M.SIM		21/7/22				
Work Area				DESIGN								
				DESIGN CHECK						PREPARED FOR		
				TRAFFIC MNGR						Systems Connect		
										ISSUE STATUS		
										SHEET No.		
										2 of 2		
										ISSUE		
										0		



LEGEND			NOTES		PLOT DATE / TIME		PLOT BY M SIM		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.			A3
 Traffic direction		SCALES ON A3 SIZE DRAWING		DRAWINGS / DESIGN PREPARED BY		TITLE	NAME	DATE			Mowbray Road eastbound Lane 1 closure and Lane 2 rolling block SHEET			
						DRAWN	M.SIM	21/7/22						
						DRG CHECK	M.SIM	21/7/22						
 Temporary concrete barrier		CO-ORDINATE SYSTEM MGA ZONE 56		HEIGHT DATUM AHD		DESIGN			PREPARED FOR Systems Connect		ISSUE STATUS		SHEET No. 1 of 2	ISSUE 0
						DESIGN CHECK								
						TRAFFIC MNGR								
 Work Area														

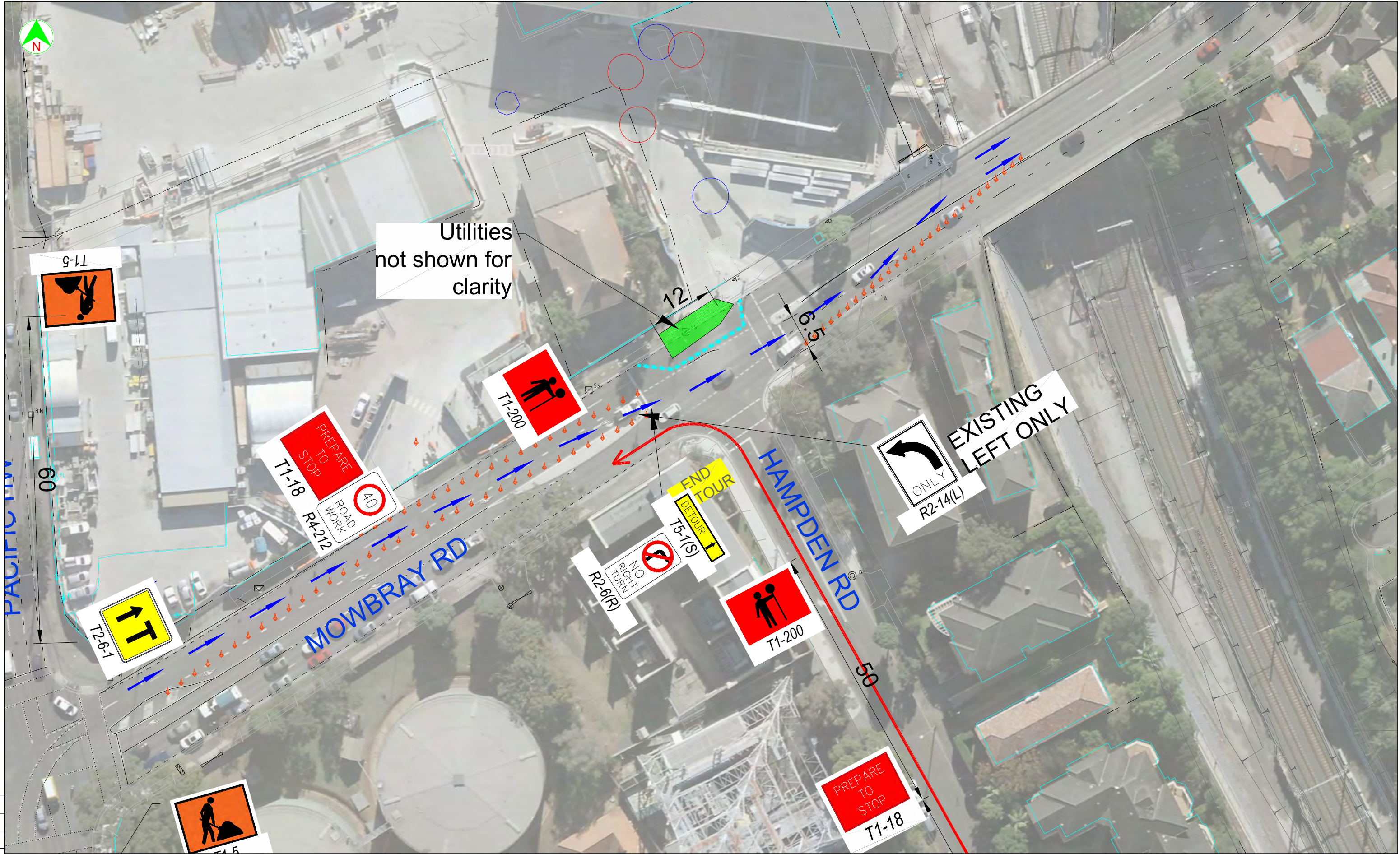


LEGEND			NOTES		PLOT DATE / TIME		PLOT BY	CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.			A3		
							M SIM								
Traffic direction					DRAWN		M.SIM							21/7/22	
					DRG CHECK		M.SIM							21/7/22	
					DESIGN				PREPARED FOR		Mowbray Road eastbound Lane 1 closure and Lane 2 rolling block SHEET				
					DESIGN CHECK										
					TRAFFIC MNGR										
Temporary concrete barrier									Systems Connect		ISSUE STATUS		SHEET No.		ISSUE
Work Area			CO-ORDINATE SYSTEM		HEIGHT DATUM						2 of 2		0		
			MGA ZONE 56		AHD										

1.6.2 Option A



			NOTES	PLOT DATE / TIME		PLOT BY M SIM	CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.			A3		
LEGEND			SCALES ON A3 SIZE DRAWING	DRAWINGS / DESIGN PREPARED BY		TITLE							NAME	DATE
<div><div></div> Detour Mowbray Rd westbound traffic</div> <div><div></div> Detour Hampden Rd northbound traffic</div> <div><div></div> Traffic direction</div> <div><div></div> Work Area</div>						DRAWN							M.SIM	21/7/22
						DRG CHECK	M.SIM	21/7/22						
						DESIGN								
						DESIGN CHECK			PREPARED FOR					
						TRAFFIC MNGR			Systems Connect		ISSUE STATUS	SHEET No.	ISSUE	
											1 of 2		0	



LEGEND		NOTES		PLOT DATE / TIME		PLOT BY		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.		A3
— Detour Mowbray Rd westbound traffic		DRAWINGS / DESIGN PREPARED BY		TITLE		NAME		DATE		Option A - Detour		SHEET
— Detour Hampden Rd northbound traffic				DRAWN		M.SIM		21/7/22				
— Traffic direction				DRG CHECK		M.SIM		21/7/22				
Work Area				DESIGN								
				DESIGN CHECK						PREPARED FOR		
				TRAFFIC MNGR						Systems Connect		
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1.6.3 Option B

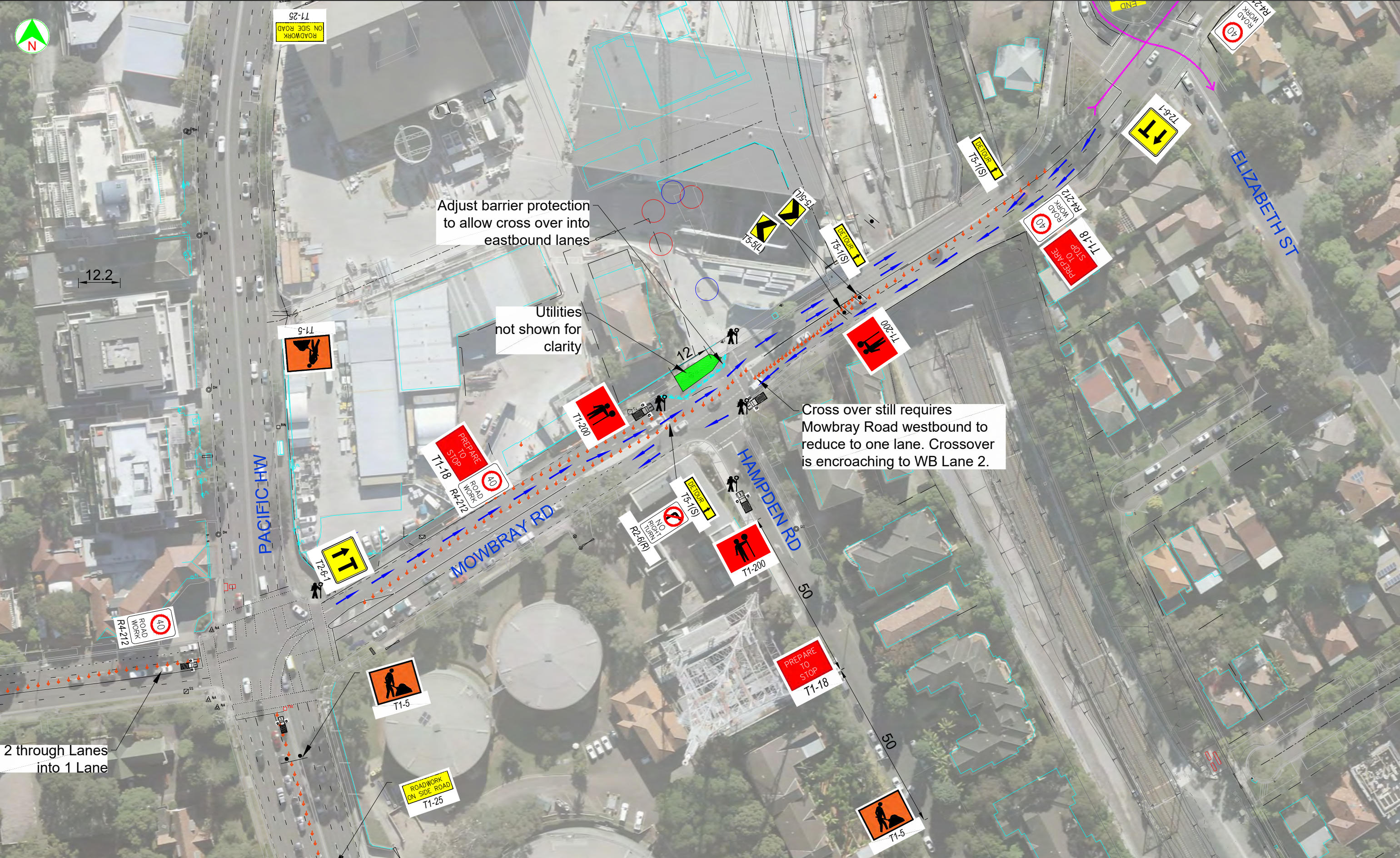


LEGEND		NOTES		PLOT DATE / TIME		PLOT BY		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.		A3
Traffic direction		DRAWINGS / DESIGN PREPARED BY		TITLE		NAME		DATE		Option B - Contraflow		SHEET
Detour for Hampden Rd southbound traffic ONLY				DRAWN		M.SIM		21/7/22				
Temporary concrete barrier				DRG CHECK		M.SIM		21/7/22				
				DESIGN								
				DESIGN CHECK						PREPARED FOR		
Work Area				TRAFFIC MNGR						Systems Connect		
										ISSUE STATUS		
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1.6.4 Option C

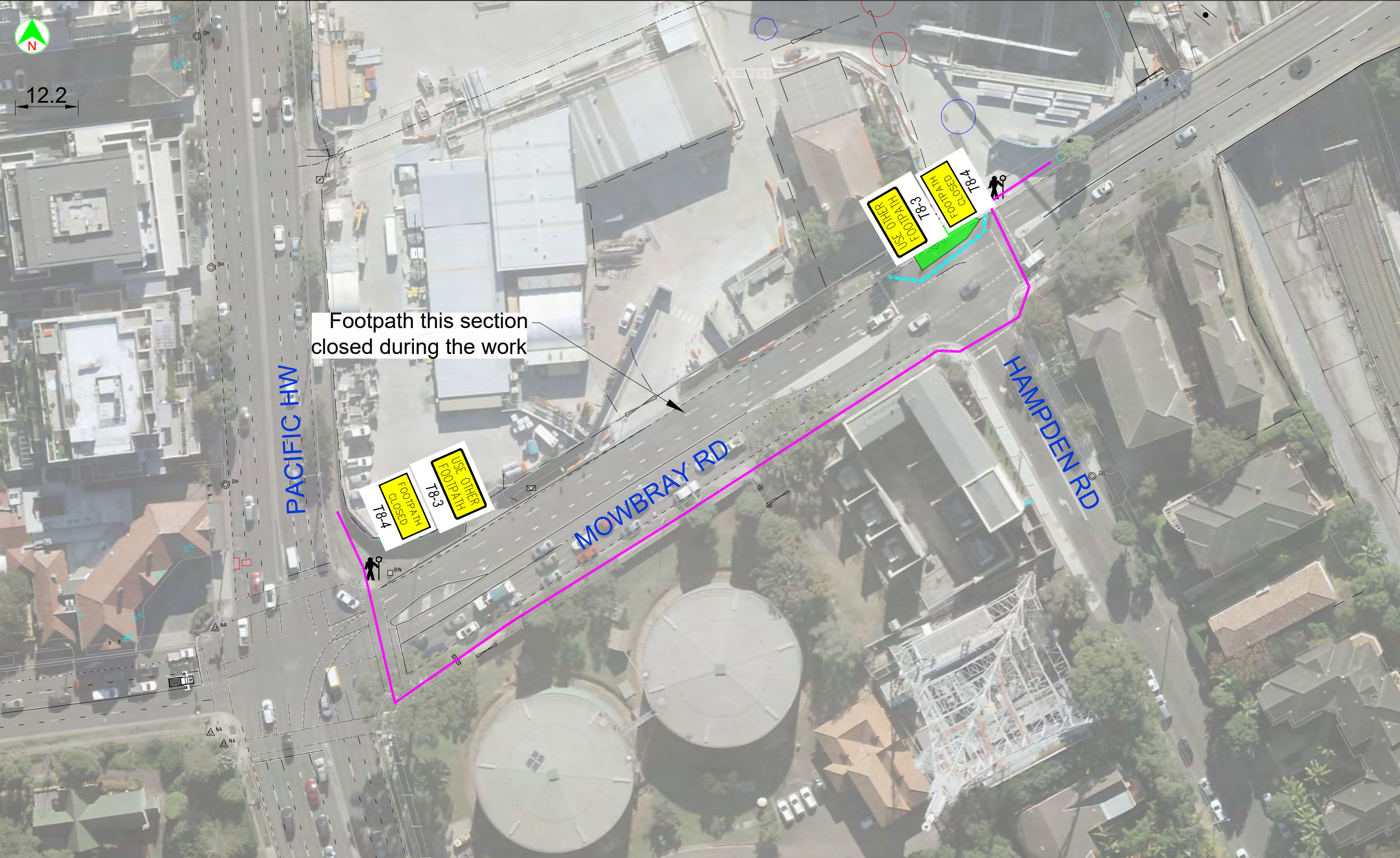





LEGEND			NOTES		PLOT DATE / TIME		PLOT BY		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.		A3
Traffic direction			DRAWINGS / DESIGN PREPARED BY		TITLE		NAME		DATE		Option C- Cross-over		SHEET
Detour for Hampden Rd southbound traffic ONLY					DRAWN		M.SIM		21/7/22				
Temporary concrete barrier					DRG CHECK		M.SIM		21/7/22				
					DESIGN								
					DESIGN CHECK								
Work Area					TRAFFIC MNGR						Systems Connect		
											ISSUE STATUS		
											SHEET No.		
											1 of 2		
											ISSUE		
											0		



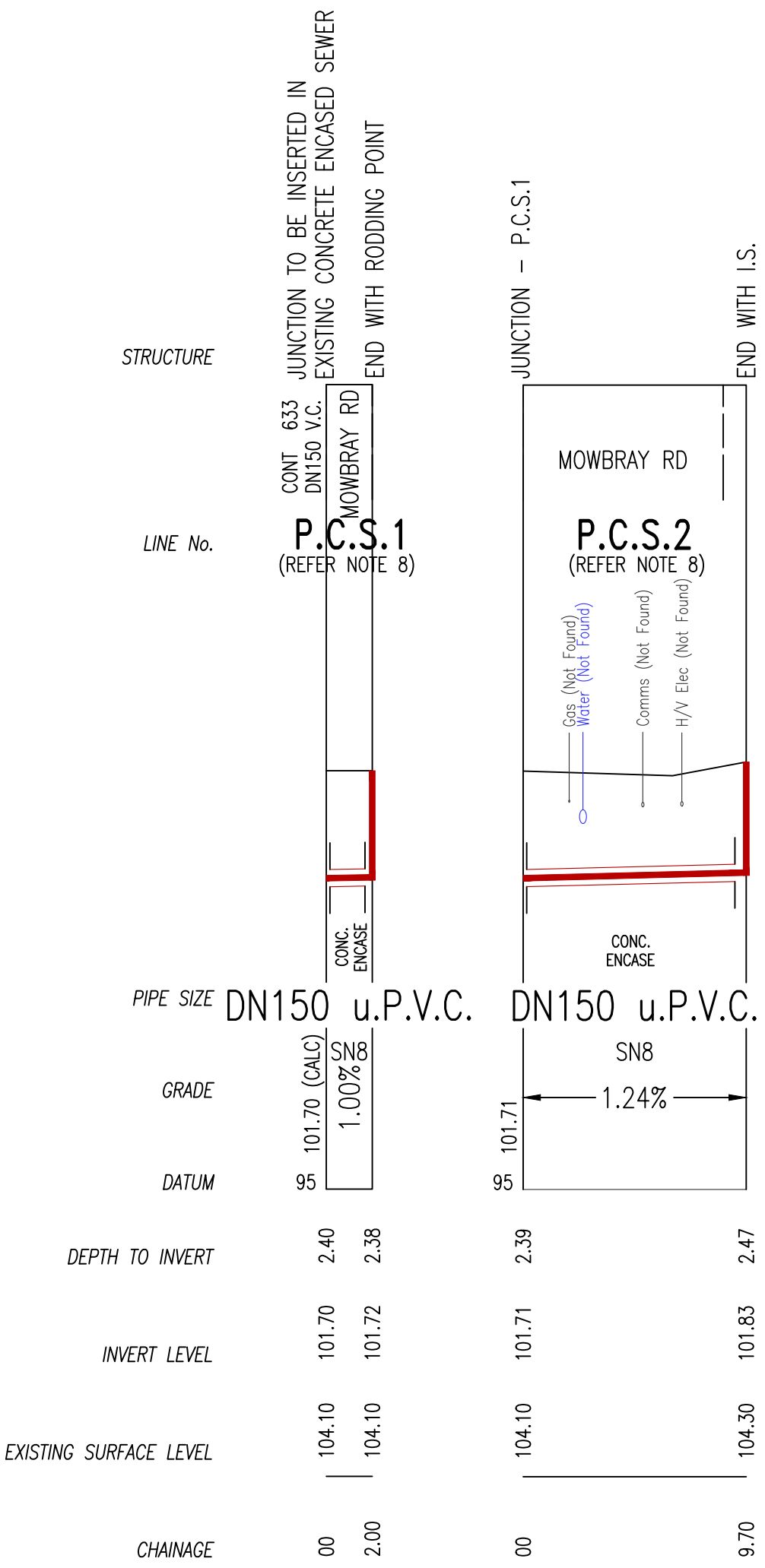
LEGEND			NOTES		PLOT DATE / TIME		PLOT BY		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.		A3
Traffic direction			DRAWINGS / DESIGN PREPARED BY		TITLE		NAME		DATE		Option C - Cross - Over		SHEET
Detour for Hampden Rd southbound traffic ONLY					DRAWN		M.SIM		21/7/22				
Temporary concrete barrier					DRG CHECK		M.SIM		21/7/22				
					DESIGN								
					DESIGN CHECK						PREPARED FOR		
Work Area					TRAFFIC MNGR						Systems Connect		
											ISSUE STATUS		
											SHEET No.		
											2 of 2		
											ISSUE		
											0		

1.6.5 Pedestrian Plan



LEGEND			NOTES		PLOT DATE / TIME		PLOT BY M SIM		CLIENT		Mowbray Road Utility Work - Sewer and Water Supply to Northern Connection.			A3			
			DRAWINGS / DESIGN PREPARED BY		TITLE	NAME	DATE										
					DRAWN	M.SIM	11/8/22										
					DRG CHECK	M.SIM	11/8/22										
 Pedestrian Detour										PREPARED FOR		Pedestrian Plan due to footpath closed during the work			SHEET		
 Work Area																	
 Temporary concrete barrier																	
			CO-ORDINATE SYSTEM MGA ZONE 56		HEIGHT DATUM AHD						Systems Connect		ISSUE STATUS		SHEET No. 1 of 1		ISSUE 0


1.6.6 Design Drawings



- WATER SERVICING COORDINATOR: ROSE ATKINS RIMMER (INFRASTRUCTURE) Pty. Ltd.
SHOP 7 & 8 M CENTRE
40 STERLING ROAD MINDCHINBURY NSW 2770
Ph: (02) 9853 0200
- FOR: TRANSPORT FOR NSW (SYDNEY METRO)
P.O. BOX K659 HAYMARKET NSW 1240
Ph: 0435 106173
2. ALL WORKS & MATERIALS ARE TO BE IN ACCORDANCE WITH THE SEWERAGE CODE OF AUSTRALIA WSA 02-2002-2.2 (SYDNEY WATER EDITION - Version 4).
3. ALL SERVICES SHOWN ARE INDICATIVE ONLY. A CURRENT SERVICES SEARCH & SITE CHECK OF ALL EXISTING SERVICES WILL BE REQUIRED PRIOR TO COMMENCEMENT OF ANY WORKS.
4. THE CONSTRUCTOR IS TO DETERMINE LEVELS & LOCATIONS OF SERVICES PRIOR TO CONSTRUCTION.
5. THE CONSTRUCTOR SHALL VERIFY EXISTING INVERT LEVELS PRIOR TO CONSTRUCTION.
6. PROPERTY CONNECTION SEWERS TO BE INSTALLED IN ACCORDANCE WITH DTC-2120 AND MWS-103. SIZE AND FINAL LOCATION TO BE CONFIRMED ON SITE WITH HEAD PLUMBING CONTRACTOR.
7. PUMP TO SEWER CONNECTION APPROVAL VIA TAP-IN - REF: 1103303.
8. NO EXCAVATION WORK OR GROUND DISTURBANCE TO OCCUR OUTSIDE DRIVEWAY ZONE OR WITHIN 10m EXCLUSION ZONE OF MOWBRAY HOUSE DUE TO HERITAGE STATUS.



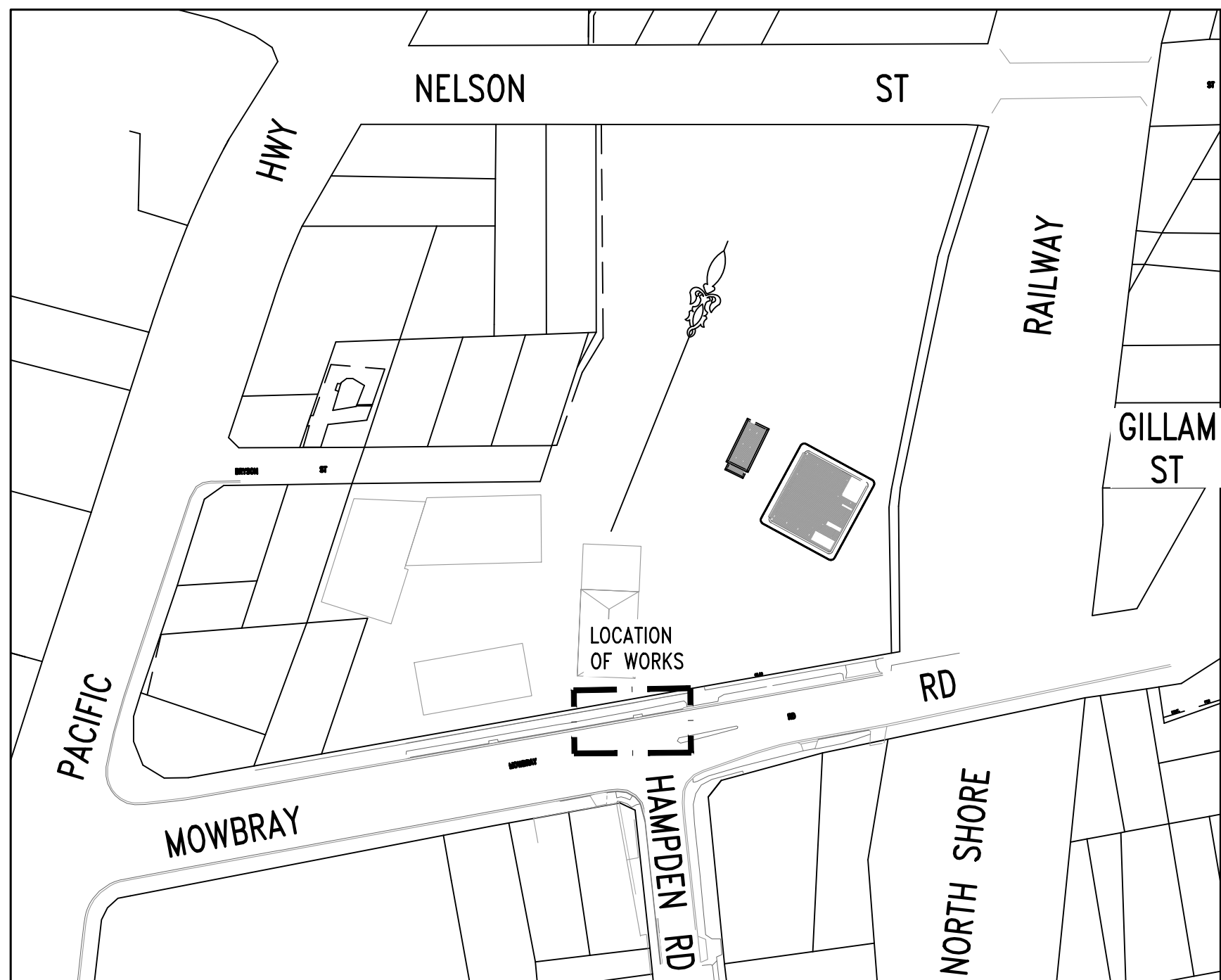
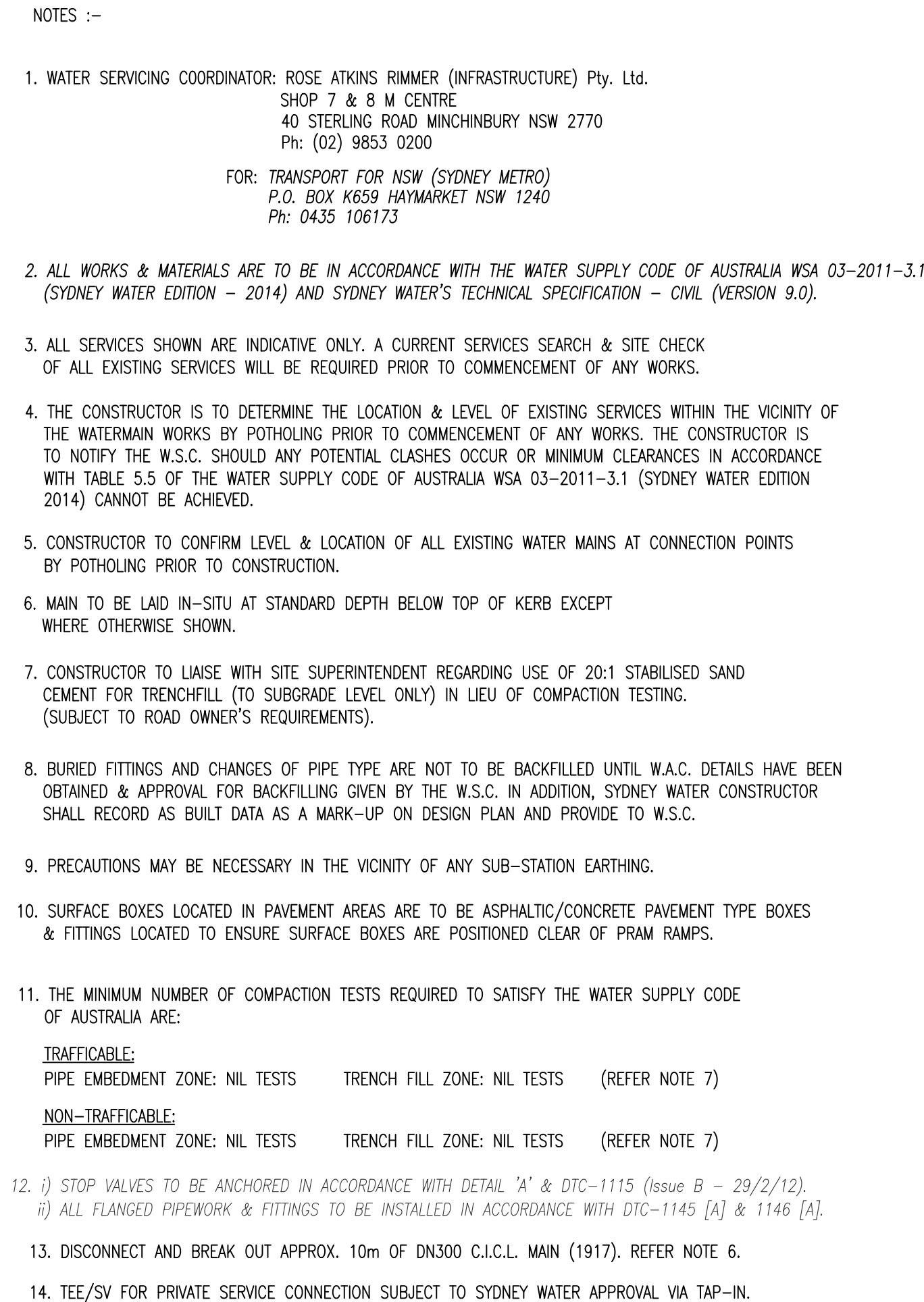
SYDNEY METRO BSF CHATSWOOD



SYDNEY WATER CORPORATION

Case No. 183714WW

WILLOUGHBY SEWERAGE
DRAINS TO
SCOTTS CREEK SUBMAIN
N.S.O.O.S.



LOCALITY PLAN
(NOT TO SCALE)

SYDNEY METRO BSF CHATSWOOD

PLAN TO BE READ IN CONJUNCTION WITH CURRENT SYDNEY WATER STANDARDS SYDNEY WATER CORPORATION		UTILITIES					
		TYPE	DATE	REF.	TYPE	DATE	REF.
STORMWATER: _____ SWD _____		26/03/21	DBYD				
TELECOMMS: _____ COMMS _____		26/03/21	DBYD				
AUSGRID: _____ E _____		26/03/21	DBYD				
GAS: G(MP) _____ G(MP) _____		26/03/21	DBYD				
DIAL BEFORE YOU DIG Ph. 1100							
ELECTRICITY _____ AUSGRID _____	Ph. 131 388						
GAS _____ JEMENA _____	Ph. 131 909						
TELECOMMUNICATIONS _____ TELSTRA _____	Ph. 132 203						
GIVING AT LEAST 48 HOURS NOTICE.							

WORK AS CONSTRUCTED CERTIFICATION	
DEVELOPER	
WATER SERVICE CO-ORDINATOR	
CONSTRUCTOR	
COMPLETED	W.A.C. PREPARED
DESIGNER	
I CERTIFY THAT THE WORKS HAVE BEEN CONSTRUCTED IN ACCORDANCE WITH THE WORK AS CONSTRUCTED DRAWINGS.	

PIPE SCHEDULE				
SIZE (DN)	TYPE	CLASS	LENGTH	PIPE JOINING METHOD / NOTES
300	D.I.C.L.	PN35	2.0	
300	D.I.C.L.	FL	7.3	
DESIGN HEAD, 120 m			BOUNDARY TRAPS REQUIRED.	

GDA 2020

AUSTRALIAN HEIGHT DATUM


SCALES

PLAN 1:250 SECTION { HOR. . . .
 { VERT. . . .

CROSS SECTIONS . . . NATURAL

LENGTHS, DEPTHS & LEVELS ARE IN METRES.

NO AMENDMENTS ARE TO BE MADE TO THIS PLAN WITHOUT REFERENCE TO SYDNEY WATER. THIS PLAN IS NOT NECESSARILY UP TO DATE OR CORRECT AND SYDNEY WATER ACCEPTS NO RESPONSIBILITY.	
U.B. DIRECTORY	MAP 195 J13 (Version 8.1 Digital)
SHEET. 1 . OF. 1 .	File No. N/A

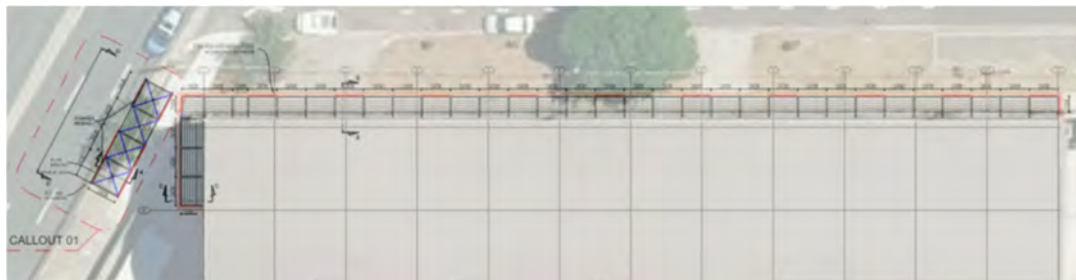
	SYDNEY WATER CORPORATION
Case No. 183714PW	
WATERMAIN ADJUSTMENT	
MOWBRAY RD	
CHATSWOOD	
WILLOUGHBY	

Appendix F. Traffic Management Plan – Redundant Spoil/ Logistics Shed Dismantlement Work

Appendix F. Traffic Management Plan – Spoil Shed Removal Works

1.1 Introduction

The Northern Dive site is being demobilised and in order to facilitate the removal of the temporary spoil shed on the north-western corner of the site a scaffold encapsulating the spoil shed will be installed. This is necessary to eliminate the risk to the public of falling debris and will require traffic management on Nelson Street and on Pacific Highway as discussed below.



These works will affect Nelson Street for around two months and the Pacific Highway on two separate occasions of 1 to 3 nights duration when Class B hoarding is erected and 1 to 3 nights duration to remove the Class B hoarding.

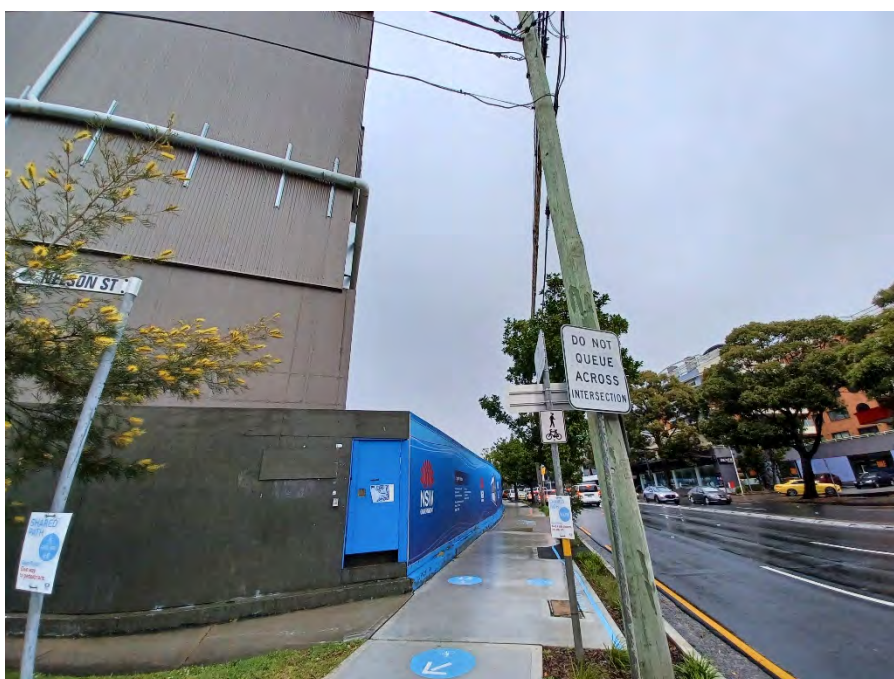
The purpose of the scaffolding is to provide a screen between the dismantlement works and the general public outside of the site. Construction vehicles removing material from the redundant building will access the site through the existing site access and egress locations on Mowbray Road (please see below excerpt from page 7 of this document). These vehicles will be a mixture of rigid and articulated heavy vehicles that will not differ from the classes of vehicles currently attending the Northern Dive site.

Willoughby Council and Sydney Buses have been consulted during planning for these works, their responses are attached in this appendix

The different traffic arrangements are provided later in this appendix. There are three different arrangements:

- Pacific Highway – Night works to erect and remove Class B hoarding – this will take place on two separate occasions of maximum 3 nights each.
- Nelson Street – Occupying southern side of Nelson Street, closure of footpath and occupation of car parks along street frontage of the site shed to be dismantled and occupation of four car parks in the indented car parking area – during erection and stripping of scaffold.
- Nelson Street - Occupying southern side of Nelson Street, closure of footpath and occupation of four car parks in the indented car parking area on the southern side of Nelson Street – this would occur during demolition works





1.2 Traffic Management for Nelson Street

Occupation of the footpath on the southern side of Nelson Street, temporary removal of parking on the southern side of Nelson Street. Traffic controllers will direct pedestrians from the Frank Channon walk to the footpath on the northern side of Nelson Street. Pedestrian access from Nelson Street to the Pacific Highway will be maintained through this route. Traffic along Nelson Street would have a reduced speed limit of 40km/h and would maintain 2way/ 2 lane. The temporary removal of parking

would be by a council permit and jersey barriers blocking the spaces. Sydney Metro signs would be in place informing the public of the temporary restrictions.

The works would not affect the cul-de-sac at the end of Nelson Street and the existing turning circle will be maintained.

1.3 Traffic Management Pacific Highway

Through traffic for northbound lanes would not be affected by this work. Occupation of one southbound lane on the Pacific Highway would maintain free-flowing traffic at all times in the remaining two lanes.

1.4 Traffic Impact Summary

Traffic management on the Pacific Highway will start from the hour where traffic volumes begin to ease from typically 10pm and work will be demobilised before traffic volumes begin to saturate from 5am. Completing the work within this time frame will reduce significantly the any risk of causing major traffic impacts. These impacts will be assessed further during application for the ROLs, and works will occur during the approved hours.

1.5 Residents

Access would be maintained at all times to all properties. The road occupation of the southern side of Nelson Street affects the frontage of the Sydney Metro site only. The cul-de-sac at the end of Nelson Street will be maintained at all times, access for garbage trucks and delivery trucks will not be affected. The project only seeks to occupy the parking areas on the southern side of Nelson Street. And the carriageways would remain as existing. Therefore, any vehicle currently accessing Nelson Street will continue to have this access. This includes garbage trucks, delivery vehicles and other vehicles that service the residents and businesses in Nelson Street

1.6 Pedestrians

Regarding Nelson Street, pedestrian use of the Northern side of Nelson Street is unaffected by the work. Traffic controllers will direct pedestrians from the Frank Channon Walk to the Pacific Highway by this route. Regarding Pacific Highway, where pedestrians or dismounted cyclists need to pass through the work area, works will temporarily cease and traffic controllers will escort the people through

1.7 Emergency Services

The traffic management crew and work crew will prioritise all emergency vehicles that may be encountered during the road work. During the works there will be two lanes open at all times. Emergency vehicles' access to the site will be unchanged

1.8 Cyclists

Nelson Street is bounded two shared paths, the Frank Channon Walk at the eastern end of Nelson Street and the shared path along the eastern side of the Pacific Highway. The cycle path between the Frank Channon Walk and the Pacific Highway is along the carriageway of Nelson Street. During the works, the southern side of Nelson Street would be closed. This will not affect cyclists on Nelson Street as the carriageways will not be reduced in width. During the night works on the Pacific Highway, the shared path on the eastern side of the Pacific Highway will be occupied for a maximum of three nights while the Class-B hoarding is erected and three nights at a later period during Class-B hoarding dismantlement. During these periods, similarly to pedestrians, dismounted cyclists will be escorted through the work area after work temporarily pauses.

1.8 Indicative Works Program, TGS's, Design Drawings, and Stakeholder Consultation






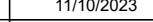

1.8.1 Indicative Works Program

Indicative Program of Shed Demolition Works

[illegible]

1.8.2.1 TGS for Nelson Street During Scaffold Work – TGS 1

Step 1 - Consequence (impact)				
Negligible (1)	Minor (2)	Moderate (3)	Major (4)	Severe (5)
First Aid Treatment	Medical Treatment	Lost Time Injury	Permanent Impairment Injury	Fatality
Very minor injury that requires no treatment or simple first aid	Injury / illness, which requires medical treatment and may temporarily restrict a persons capacity to work	Injury / illness, which temporarily renders a person unfit to work in any capacity	Injury / illness, which permanently alters a persons future (eg. Spinal injury, amputation or death)	
Short term damage	Limited but medium term damage	Significant but recoverable ecological damage	Heavy ecological damage, costly restoration	Permanent widespread ecological damage
Brief delay / slight impact on service delivery	Local or worksite specific impact on service delivery or customer satisfaction	Temporary impact on service delivery or customer satisfaction at a local event / project level	Serious impact on service delivery or customer satisfaction at a state client or large project level	Long term or very severe impact on service delivery or customer satisfaction resulting in loss of business nationally
Moderate (8)	High (16)	High (18)	High (21)	Extreme (25)
Moderate (7)	Moderate (10)	High (17)	High (20)	High (24)
Low (3)	Moderate (9)	Moderate (12)	High (19)	High (23)
Low (2)	Low (5)	Moderate (11)	Moderate (14)	High (22)
Low (1)	Low (4)	Low (6)	Moderate (13)	Moderate (15)
Step 3 - The risk rating is where the consequence and the probability intersect				

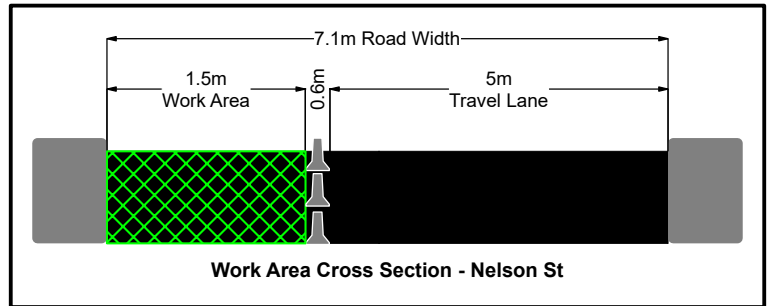
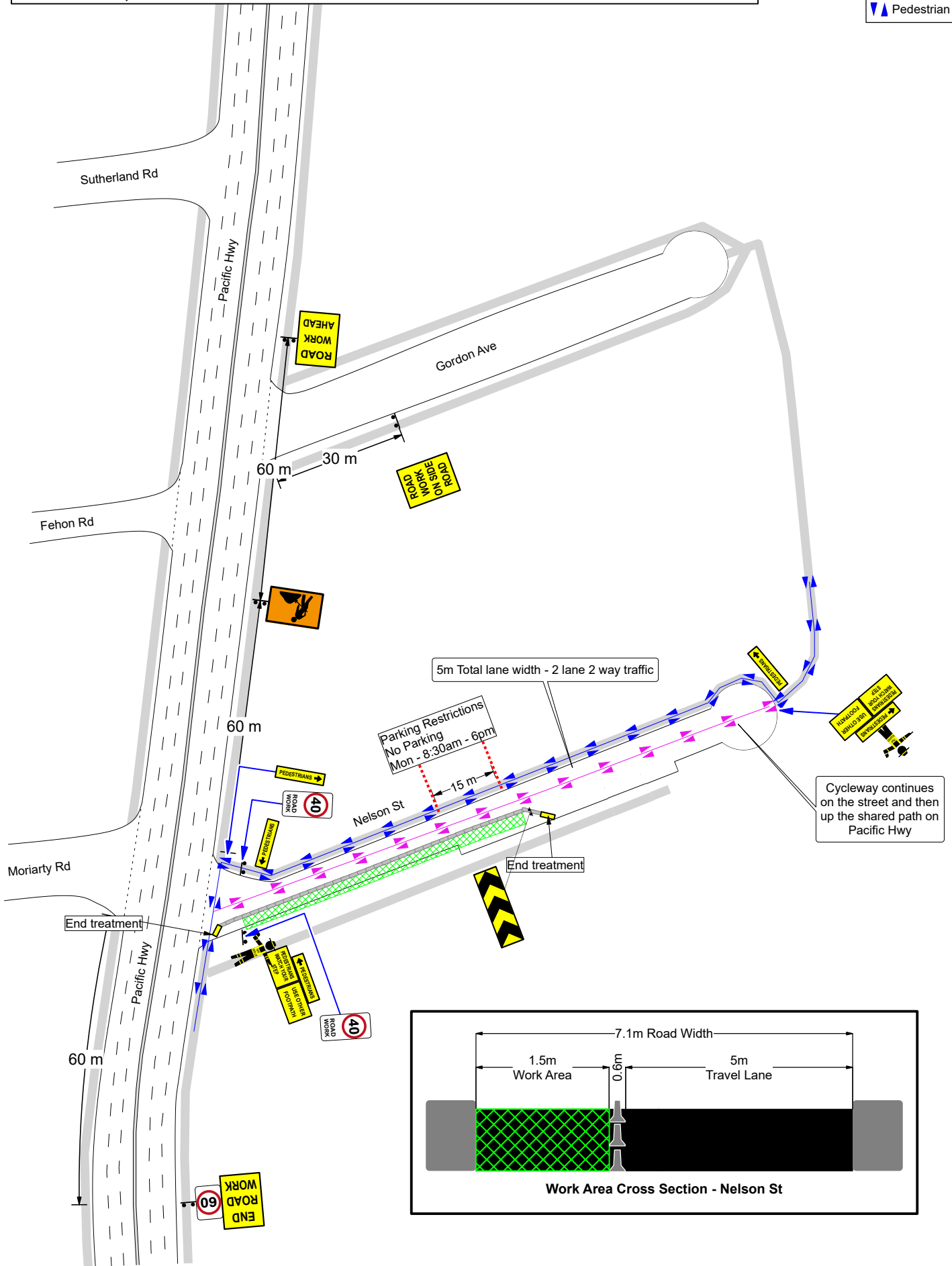
ne	Desg	Appd	Date & Time	Amendment Description	TGS Name & Number:		TGS Designed By: Karisa Dwyer		PWZTMP: TCT0039102	Exp: N/A	Signature: 	Date of Approval:	Page 2
	KD	PL	10.10.23 - 1:15am	Original Issue	62313: Chatswood — Nelson St, Chatswood NSW 2067, Australia		TGS Approved By: Peter Lozano		PWZTMP: TCT0058486	Exp: N/A	Signature: 	11/10/2023	
					Works Location:		Client Company: Systems Connect		Client:   				
				Nelson St, Chatswood NSW 2067, Australia		Client Contact: Jason Austin		Contact Number: 0428428146					
				Project Name:		Project Description:							
				Southwest Line-wide Works		Nelson St Should Closure							
Scale: 1 : 750				Original Size A3		Lack Group acknowledges the traditional owners of country throughout Australia and recognises their continuing connection to land, waters and community. We pay our respect to them and their cultures; and to elders both past and present.							

TRAFFIC CONTROL FOR PERIODS WHEN SCAFFOLDING IS BEING ERECTED, MODIFIED AND DISMANTLED

Legend

▲ Cyclist Path

▲ Pedestrian Path



Date of works: Start time of works: End time of works: (24 hour time)

Issue	Desg	Appd	Date & Time	Amendment Description	TGS Name & Number:	TGS Designed By:	PWZTMP:	Exp:	N/A	Signature:	Date of Approval:	Page 4 / 4
01	KD	PL	10.10.23 - 1:15am	Original Issue	62313: Chatswood — Nelson St, Chatswood NSW 2067, Australia	TGS Approved By:	Peter Lozano	PWZTMP:	TCT0058486	Exp:	N/A	Signature:
02					Works Location:	Client Company:	Systems Connect	Client Contact:	Jason Austin	Contact Number:	0428428146	
03					Project Name:	Project Description:						
04					Southwest Line-wide Works	Nelson St Should Closure						
05												








Scale: 1 : 750






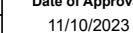

Original Size A3

Lack Group acknowledges the traditional owners of country throughout Australia and recognises their continuing connection to land, waters and community. We pay our respect to them and their cultures; and to elders both past and present.



Step 1 - Consequence (impact)				
Negligible (1)	Minor (2)	Moderate (3)	Major (4)	Severe (5)
First Aid Treatment	Medical Treatment	Lost Time Injury	Permanent Impairment Injury	Fatality
Very minor injury that requires no treatment or simple first aid	Injury / illness, which requires medical treatment and may temporarily restrict a persons capacity to work	Injury / illness, which temporarily renders a person unfit to work in any capacity	Injury / illness, which permanently alters a persons future (eg. Spinal injury, amputation or death)	
Short term damage	Limited but medium term damage	Significant but recoverable ecological damage	Heavy ecological damage, costly restoration	Permanent widespread ecological damage
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Moderate (8)	High (16)	High (18)	High (21)	Extreme (25)
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Low (3)	Moderate (9)	Moderate (12)	High (19)	High (23)
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Low (1)	Low (4)	Low (6)	Moderate (13)	Moderate (15)
Step 3 - The risk rating is where the consequence and the probability intersect				

e	Desg	Appd	Date & Time	Amendment Description	TGS Name & Number:	TGS Designed By: Karisa Dwyer	PWZTMP: TCT0039102	Exp: N/A	Signature: 	Date of Approval:	Page 2
	KD	PL	10.10.23 - 1:55am	Original Issue	62313-2: Chatswood — Nelson St, Chatswood NSW 2067, Australia	TGS Approved By: Peter Lozano	PWZTMP: TCT0058486	Exp: N/A	Signature: 	11/10/2023	
					Works Location:	Client Company: Systems Connect			Client: 		
					Nelson St, Chatswood NSW 2067, Australia	Client Contact: Jason Austin	Contact Number: 0428428146				
					Project Name:	Project Description:				 	
					Southwest Line-wide Works	Nelson St Should Closure					

	TGS Designed By: Karisa Dwyer		PWZTMP: TCT0039102	Exp: N/A	Signature: 	Date of Approval: 11/10/2023	Page 2
	TGS Approved By: Peter Lozano		PWZTMP: TCT0058486	Exp: N/A	Signature: 		
	Client Company: Systems Connect			Client:   			
	Client Contact: Jason Austin		Contact Number: 0428428146				

Item #	Worksite Component	Potential Hazard	Initial Risk			Present	Control Measures	Residual Risk		
			C	P	R			C	P	R
6.4	Bus stops	Bus unable to pull up safely causing MVA	3	3	12	Y	<ul style="list-style-type: none"> - Consider notifying bus companies that operate in the area - Always provide adequate provision for buses or carry out work at night when buses aren't operating - Where temporary bus stops are created, ensure buses are able to meet the curb - Ensure TGS clearly shows affected stops - Traffic controllers to manage and assist where safe and possible 	2	2	5
6.5	Property accesses - commercial or private	Collisions due to propertie aces restrictions	3	4	17	Y	<ul style="list-style-type: none"> - Consider staging work outside of business hours - Create physical barrier to prevent traffic entering site & driveways 	2	2	5
6.6	Excavations within work area	Errant vehicle drives into excavation	5	4	25	Y	<ul style="list-style-type: none"> - For excavations shallower than 0.5m and within 3m of the edge of traffic lane, delineate the excavation with plastic mesh fencing, barrier boards placed perpendicular to the traffic flow or cones/bollards. - For excavations deeper than 0.5m and within 3m of the edge of traffic lane, a temporary safety barrier must be installed. When traffic is greater than 3m from the excavation, the requirement for a temporary safety barrier should be considered based on a documented risk assessment. - Where the excavation is deeper than 200mm, is open for more than 2 weeks and the distance from the edge of traffic lane is less than 3m for 60km/h, 6m for 80km/h and 9m for 100km/h, a temporary safety barrier must be installed. 	4	2	14
6.7	Parking	Parked vehicle or worker exiting vehicle hit by passing vehicle	4	4	20	Y	<ul style="list-style-type: none"> - Always check adequate parking is available for workers and visitors - Consider providing safe parking within the work area 	4	2	14
6.8	Concurrent Works	Motorist confused by conflicting signs causing MVA	3	4	17	Y	<ul style="list-style-type: none"> - Always establish communication with other site if possible - Always cover any conflicting signs and adjust TGS as necessary - Complete conflict checks where required 	3	3	12
6.9	Heavy Vehicles and OSOM Vehicles	HV cannot travel past work site without knocking over delineation	4	4	20	Y	<ul style="list-style-type: none"> - Comply with shoulder and lane width criteria in the design of the TGS. - During the design of the TGS, check vehicle swept path where necessary to ensure the largest known vehicle travelling through the work site can negotiate the changed traffic conditions. - Traffic controllers to communicate with heavy vehicle and OSOM drivers to warn and guide them through the work site as required. - Traffic control to monitor heavy vehicle movements and if required, make adjustments to the signs and devices within approved tolerances. If more significant changes are required, liaise with Client/Supervisor and arrange for TGS to be reviewed and modified by the designer. 	4	2	14
Dynamic Works										
7.0	General Traffic	Motorists speeding / not concentrating / tired / distracted. Not having enough time to merge causing MVA	5	5	25	NA	<ul style="list-style-type: none"> - Always use a minimum 1 AWW and consider the use of a 2nd AWW. - Consider use of TMA on higher speed roads >85km - Use speed reduction best suited to work activity and road environment - Use applicable AW signage displayed on AWW - Ensure sight distances between AWW, shadow vehicles are clearly labelled on TGS - Ensure 20-40m buffer zone between shadow vehicle and work vehicle. No less than 40m when using a TMA as a shadow vehicle - Positive communications to be held at all times - Workers to remain shadowed at all times - Monitor traffic queues on all road configurations, convoy to clear roadway if required until traffic has cleared 	4	2	14

Item

Additional Control Control Measures

8.0

9.0

10.0

11.0

Item

Departures: State the departure and reason for departure

12.0

13.0

14.0

Departures Sign Off (CLIENT):

Client Name:

Client Signature:

Date:

NOTES:

GENERAL NOTES

1. This Traffic Guidance Scheme (TGS) is to be used in conjunction with the Traffic Management Plan (TMP) and associated road authority permits and management plans, including Road Occupancy Licence (ROL), vehicle movement plan (VMP) and pedestrian movement plan (PMP) where applicable.
2. This TGS has been produced by a Prepare Work Zone Traffic Management Plan (PWZTMP) qualified person in accordance with the requirements of the TfNSW Traffic Control at Work Sites manual, Issue 6.1 dated 28 February 2022 (TCAWS 6.1) and with reference to AS1742.3 and AUSTRROADS Guide to Temporary Traffic Management Parts 1 – 10, version 1.1 dated September 2021 (AGTTM).
3. This TGS is suitable for short term/ long term works.
4. Lack Group does not accept responsibility for this TGS if it is implemented or modified by external parties.

APPROVALS

5. The TGS must be approved for use before implementation.
6. Ensure all road authority approvals and associated conditions of approval are met prior to implementing the TGS.

TGS VERIFICATION

7. Prior to use on site, the selected or designed TGS must be verified to ensure it is suitable for the works and location by undertaking an inspection of the work site where the TGS will be implemented. The TGS verification must be completed in accordance with TCAWS 6.1, Section 8.1.2 by an Implement Traffic Control Plan (ITCP) or PWZTMP qualified person. Refer Page 1 of this TGS for Site Verification sign-off.

RISK ASSESSMENT

8. A desktop risk assessment has been undertaken in developing this TGS. However, when implementing this TGS on site, the site supervisor should undertake a site specific risk assessment to ensure that the TGS has considered and mitigated all identified hazards and risks.

INSTALLATION AND REMOVAL OF SIGNS AND DEVICES

9. All traffic management signs and devices prescribed for use in this TGS are in accordance with TCAWS 6.1 with reference to AS1742.3 and AGTTM.
10. The TGS must be installed, maintained and removed in a planned and safe manner. The implementation must only be undertaken by an ITCP qualified person.
11. All signage shown on this TGS is not to conflict with any long-term existing signage arrangements in the area. If this occurs, cover all conflicting road signage where required.

PLACEMENT OF SIGNS AND DEVICES

12. Signs must be properly displayed and securely mounted at all times and within the line of sight of the intended road user. Regulatory and detour signs must be located nearest to the travel edge of the lane. Signs must not: Be obscured from view, such as by vegetation; or parked cars; Obscure other devices from the line of sight of the intended road users; Create a hazard to road workers and road users, including pedestrians and cyclists; Be a hazard that deflects traffic into an undesirable path; Restrict sight distance for drivers entering from side roads or streets, or private driveways; and Be installed using supports that could be a hazard if struck by a vehicle.
13. Signs mounted on frames for short-term works should be mounted a minimum 200mm from the ground to the lower edge of the sign.
14. Signs mounted on posts for long-term works in open road situations, the underside of the sign must be at least 1.5m above the level of the nearest edge of the travelled path. When installed on a kerb or footpath, the underside of the sign must be at least 2.2m above the level of the nearest edge of the travelled path.

ORIENTATION OF SIGNS

15. On the outside of a curve, the sign face must be at 0 degrees, or 'normal to traffic'. On a straight, the sign face must be angled at approximately 5 degrees normal to oncoming traffic and on the inside of a curve, the sign face must be angled at approximately 5 degrees normal to oncoming traffic at 200m preceding the sign.

TOLERANCES

16. Local constraints may not allow signage and devices to be placed in accordance with this TGS. Unless stated otherwise on the TGS, the tolerances on the positioning of signs, length of tapers or pavement markings detailed in the TGS is a minimum 10% less and a maximum 25% more than the distances or lengths stated and for the spacing of delineation devices a maximum 10% more than the spacing detailed in the TGS.
17. Any variation to the positioning of signs and devices within the approved tolerances must be marked and initialled on the TGS held on site, with the name of the person making the changes shown on the TGS.

MODIFYING TGS

18. Modifications to a Site Specific or Site Suitable TGS must be approved by a person holding the PWZTMP qualification and must be supported by a TMP or risk assessment to ensure that the TGS has considered and mitigated all identified site specific conditions and risks.
19. If it is identified that by implementing the TGS with modifications outside of the approved tolerances it will generate risks, then the works must be stopped (including the implementation of the TGS), the site must be made safe and an updated TGS must be provided by a PWZTMP qualified person prior to works recommencing. Any concerns regarding the suitability of the TGS must be raised with the Site Manager and your immediate Supervisor.

TRAFFIC CONTROLLERS

20. The implementation of traffic control must be conducted in line with the hierarchy of controls with the elimination of harm to workers and the travelling public considered in the first instance.
21. Where traffic control is required, a portable traffic control device (PTCD) must be used rather than using a manual traffic controller when the existing permanent speed limit is greater than 45 km/h.
22. TCAWS 6.1, Section 5.4 provides the conditions under which a manual traffic controller may be used.
23. Where PTCDs or traffic controllers are used, approach speeds of traffic must be reduced to less than 65 km/h.
24. All persons operating a portable traffic control device or performing manual traffic control must be qualified with 'Traffic Control' training; and authorised by the relevant road authority.

ROAD USER MANAGEMENT

25. The needs of specific road users, including travel paths and desire lines, must be considered and managed for the extent of the works to ensure safety and access is maintained. Specific road user groups to be considered include: Pedestrians including high-risk pedestrians such as persons with a disability, children, the elderly or persons using mobility aid devices; Cyclists; Motorcyclists; Heavy Vehicles, including oversize overmass vehicles; Public transport; and Emergency services. The needs of these specific road users have been considered in the design of this TGS, however the needs of all road users should be considered in the site specific risk assessment before implementing the TGS to ensure the TGS is appropriate.
26. Road users are to be monitored for the duration of the works. If additional signage and/or devices are required to manage the needs of specific road users, such as pedestrians and cyclists, this would be subject to following the procedure for modifying a TGS.

ACCESS MANAGEMENT

27. Access to properties located within the extent of works must be maintained at all times.
28. Property access impacted by the works should be identified and addressed in the TGS. Consultation with the property owner/resident must be undertaken prior to implementing the TGS if required.

INCIDENT MANAGEMENT

29. The site contractor is to determine the appropriate procedure for incident management where appropriate.
30. If an incident occurs within the extent of the traffic control arrangement: Call for assistance if incident requires (emergency services 000 or 112); Notify the work site supervisor or Team Leader immediately of any incident; Maintain effective traffic control, if necessary, relocate the traffic control station to a suitable location clear of any further danger; and Record sufficient notes of the incident, including observations, to complete an incident report.

INSPECTIONS

31. Temporary traffic management monitoring activities must be undertaken in all instances where work is being performed or aftercare is in place. This includes day and night times as required. The type of inspections and frequency are to be in accordance with TCAWS 6.1, Section 8.1.1.

REVIEW OF TGS

32. Generic TGSs must be reviewed by a PWZTMP qualified person every 12 months so that they remain appropriate. Once reviewed the date and details of the PWZTMP person must be updated on the TGS to ensure persons selecting can confirm currency.
33. All active site specific and site suitable TGS are designed for the nominated work activity and are only valid for the time period of works specified on the TGS. They must be reviewed as part of the weekly inspections as detailed in TCAWS 6.1, Section 8.1. If the work activity is intended to be longer than 12 months, then the TGS must be formally reviewed by a PWZTMP qualified person at least every 12 months and issued with the review date and the details of the person undertaking the review.

RECORD KEEPING

34. Supervisory personnel are to keep daily records of the TGS implementation including: Site specific risk assessments; Approved TGS used, including versions where modifications or updates have been made; Completed inspection checklists that have been undertaken; Records of traffic related incidents that occurred during the works; and Any other relevant document generated by the process of completing the temporary traffic management works.

TGS Designed By: Karisa Dwyer

PWZTMP: TCT0039102

Exp: N/A

Signature:

Date of Approval: 11/10/2023

Page 3 / 4

TGS Approved By: Peter Lozano

PWZTMP: TCT0058486

Exp: N/A

Signature:

Client Company: Systems Connect

Client:

Client Contact: Jason Austin

Contact Number: 0428428146

TGS Name & Number:

62313-2: Chatswood — Nelson St, Chatswood NSW 2067, Australia

Works Location:

Nelson St, Chatswood NSW 2067, Australia

Project Name:

Southwest Line-wide Works

Project Description:

Nelson St Should Closure

Scale: 1 : 750

Original Size A3

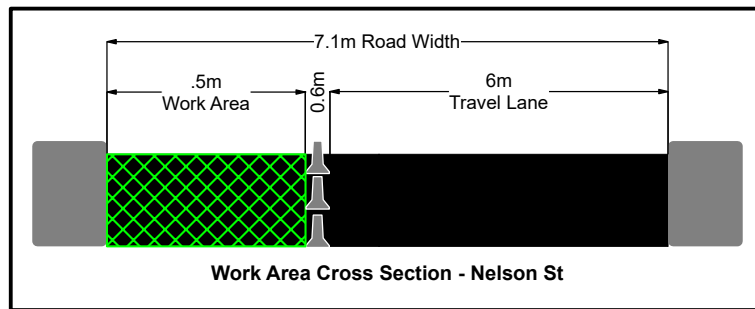
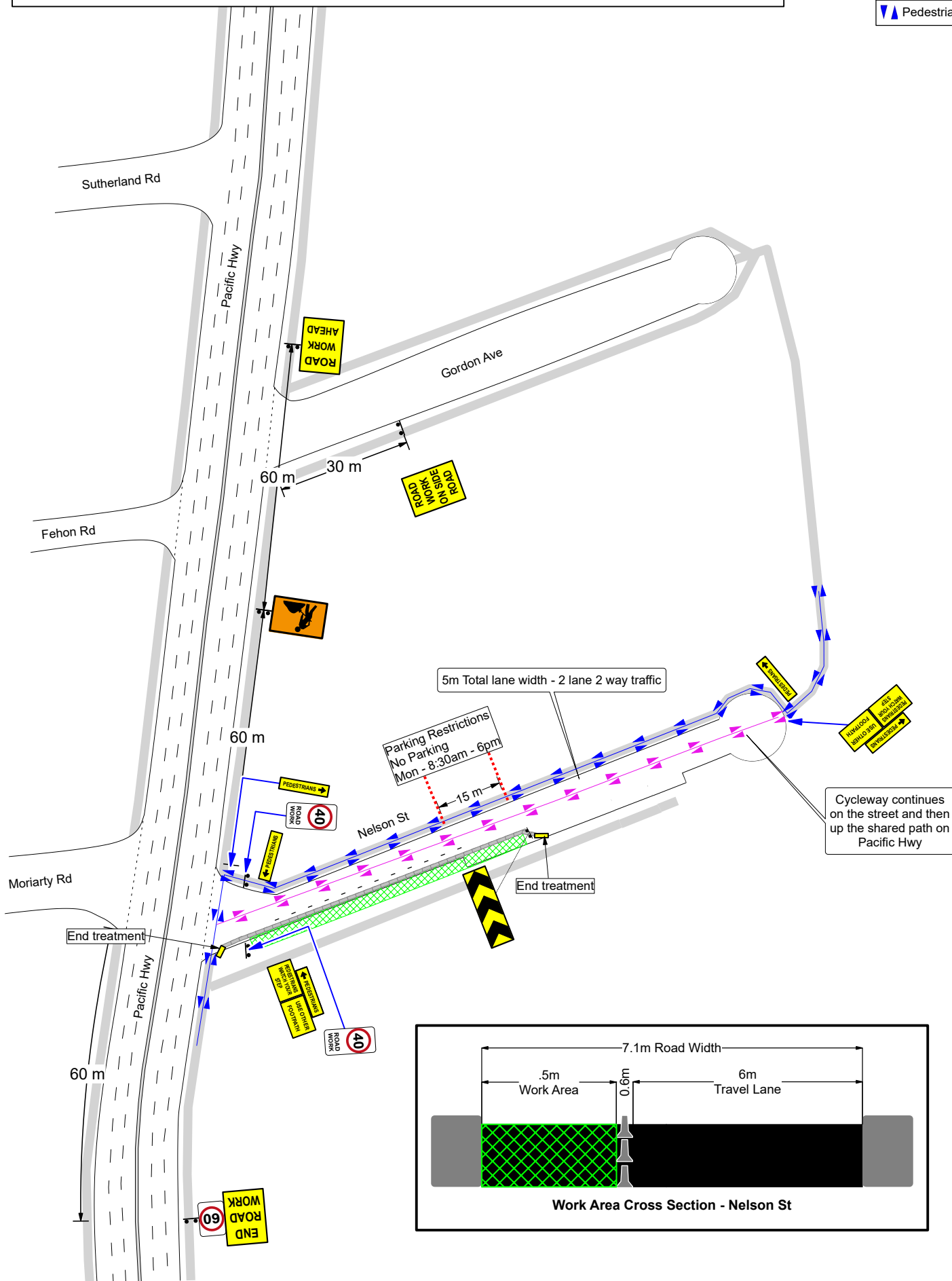
Lack Group acknowledges the traditional owners of country throughout Australia and recognises their continuing connection to land, waters and community. We pay our respect to them and their cultures; and to elders both past and present.

TRAFFIC CONTROL FOR PERIODS OUTSIDE OF SCAFFOLD WORKS

Legend

▲ Cyclist Path

▲ Pedestrian Path



Date of works: Start time of works: End time of works: (24 hour time)

Issue	Desg	Appd	Date & Time	Amendment Description	TGS Name & Number:	TGS Designed By:	PWZTMP:	Exp:	N/A	Signature:	Date of Approval:	Page 4 / 4
01	KD	PL	10.10.23 - 1.55am	Original Issue	62313-2: Chatswood — Nelson St, Chatswood NSW 2067, Australia	TGS Approved By:	Peter Lozano	PWZTMP:	TCT0058486	Exp:	N/A	Signature:
02					Works Location:	Client Company:	Systems Connect	Client Contact:	Jason Austin	Contact Number:	0428428146	
03					Project Name:	Project Description:						
04					Southwest Line-wide Works	Nelson St Should Closure						
05												

Scale: 1 : 750








Original Size A3








Lack Group acknowledges the traditional owners of country throughout Australia and recognises their continuing connection to land, waters and community. We pay our respect to them and their cultures; and to elders both past and present.

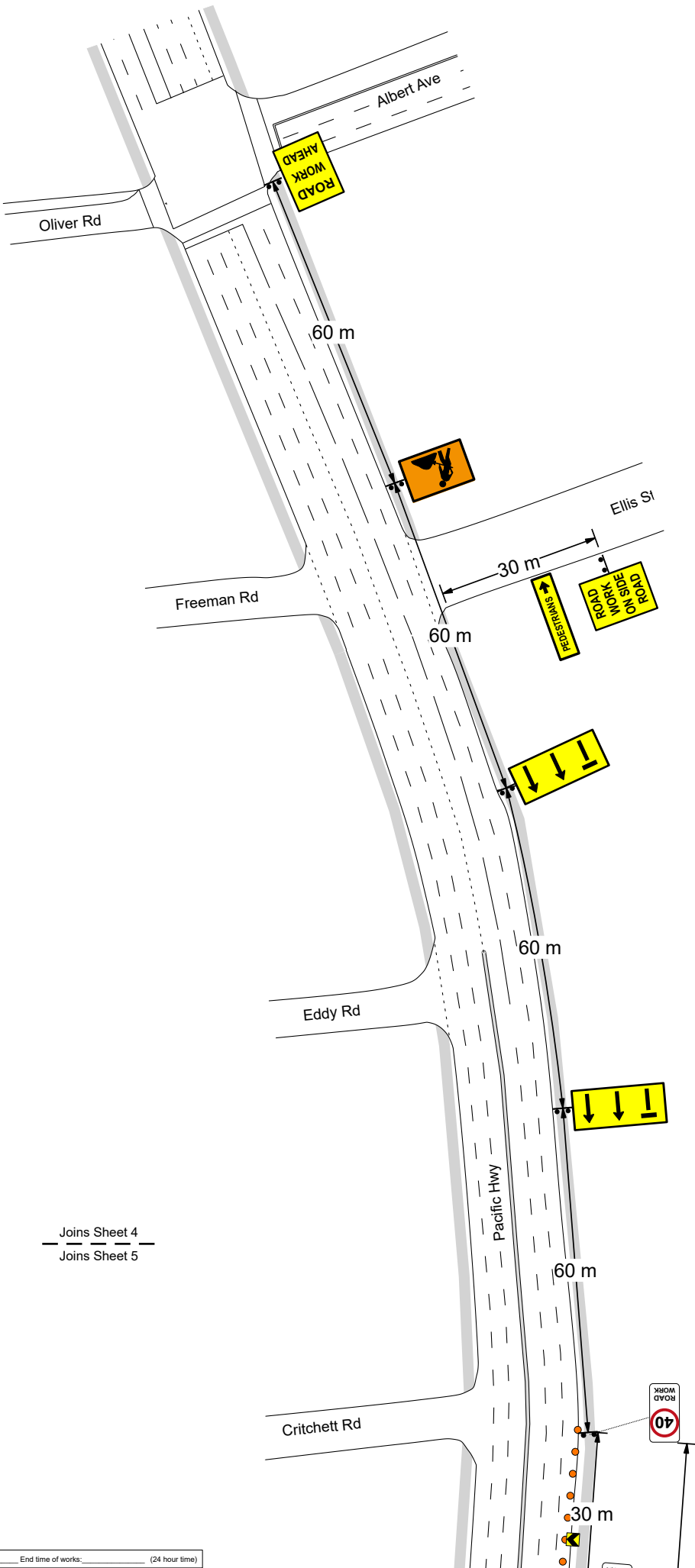


1.8.2.3 TGS for Pacific Highway – TGS 3

Step 1 - Consequence (impact)				
Negligible (1)	Minor (2)	Moderate (3)	Major (4)	Severe (5)
First Aid Treatment	Medical Treatment	Lost Time Injury	Permanent Impairment Injury	Fatality
Very minor injury that requires no treatment or simple first aid	Injury / illness, which requires medical treatment and may temporarily restrict a persons capacity to work	Injury / illness, which temporarily renders a person unfit to work in any capacity	Injury / illness, which permanently alters a persons future (eg. Spinal injury, amputation or death)	
Short term damage	Limited but medium term damage	Significant but recoverable ecological damage	Heavy ecological damage, costly restoration	Permanent widespread ecological damage
Brief delay / slight impact on service delivery	Local or worksite specific impact on service delivery or customer satisfaction	Temporary impact on service delivery or customer satisfaction at a local event / project level	Serious impact on service delivery or customer satisfaction at a state client or large project level	Long term or very severe impact on service delivery or customer satisfaction resulting in loss of business nationally
Moderate (8)	High (16)	High (18)	High (21)	Extreme (25)
Moderate (7)	Moderate (10)	High (17)	High (20)	High (24)
Low (3)	Moderate (9)	Moderate (12)	High (19)	High (23)
Low (2)	Low (5)	Moderate (11)	Moderate (14)	High (22)
Low (1)	Low (4)	Low (6)	Moderate (13)	Moderate (15)
Step 3 - The risk rating is where the consequence and the probability intersect				

e	Desg	Appd	Date & Time	Amendment Description	TGS Name & Number:		TGS Designed By: Karisa Dwyer		PWZTMP: TCT0039102	Exp: N/A	Signature: 	Date of Approval:	Page 2
	KD	PL	01.09.23 - 0702	Original Issue	LGP - TGS - 49876 - Nelson St & Pacific Hwy, Chatswood NSW 2067, Australia - 003 - R4		TGS Approved By: Peter Lozano		PWZTMP: TCT0058486	Exp: N/A	Signature: 	10/10/2023	
	PL	KD	06.09.23 - 1408	Amended as requested	Works Location:		Client Company: Systems Connect				Client:   		
	PL	KD	12.09.23 - 1132	Amended as requested	Nelson St & Pacific Hwy, Chatswood NSW 2067, Australia		Client Contact: Jason Austin		Contact Number: 0428428146				
	PL	KD	28.09.23 - 0642	Amended as requested	Project Name:		Project Description:						
	KD	PI	10.10.23 - 1:55am	Amended as requested	Southwest Line-wide Works		1 of 3 lane Takeout - Footpath Closure						
Scale: 1 : 750			Original Size A3		Lack Group acknowledges the traditional owners of country throughout Australia and recognises their continuing connection to land, waters and community. We pay our respect to them and their cultures; and to elders both past and present								

Australia - 003 - R4	TGS Designed By: Karisa Dwyer TGS Approved By: Peter Lozano	PWZTMP: TCT0039102 PWZTMP: TCT0058486	Exp: N/A Exp: N/A	Signature:  	Date of Approval: 10/10/2023	Page 2
Client Company: Systems Connect Client Contact: Jason Austin			Contact Number: 0428428146	Client:   		
Path Closure						
Australia and recognises their continuing connection to land, waters and community. We pay our respect to them and their cultures; and to elders both past and present.						



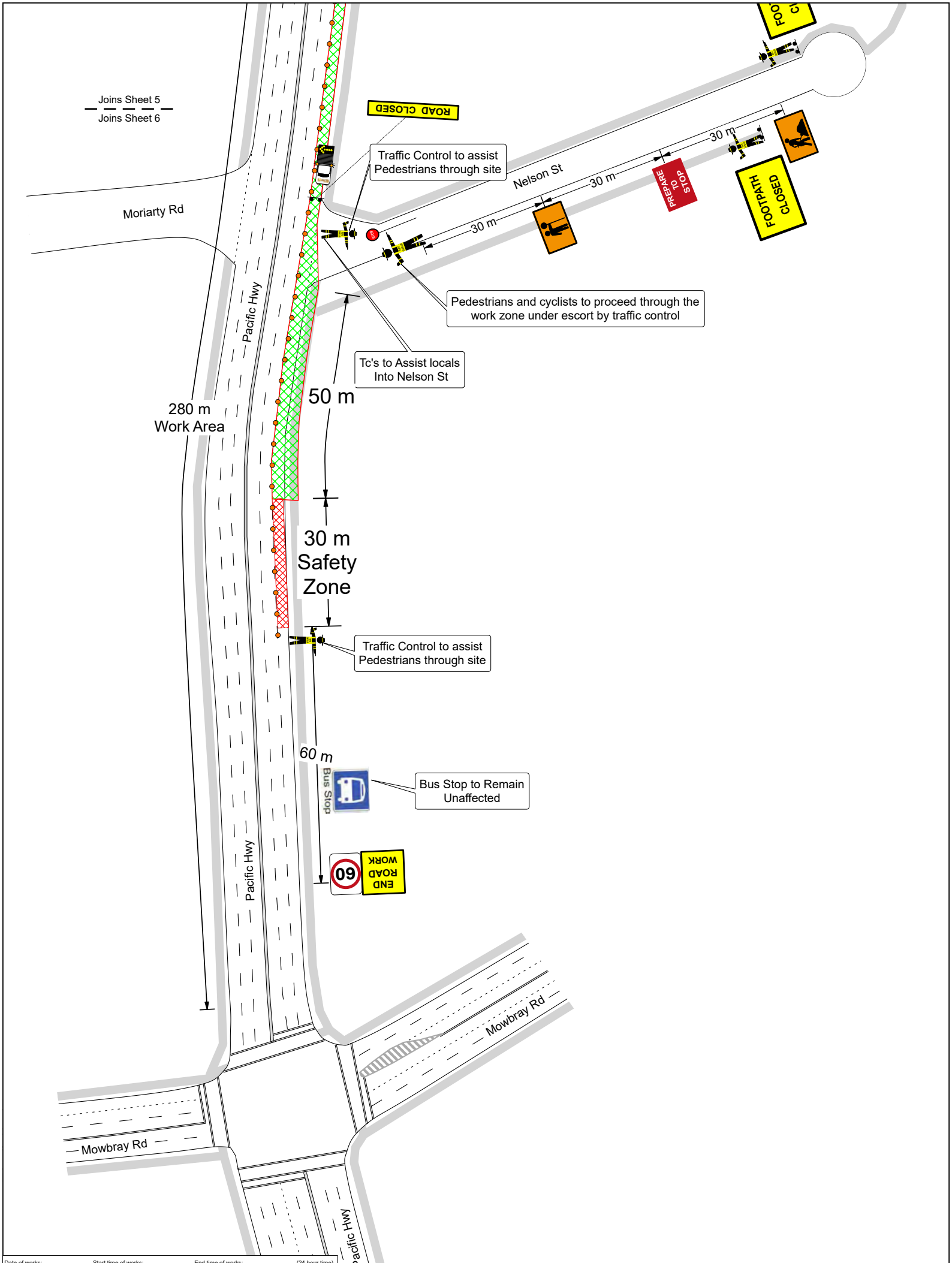
Joins Sheet 4
Joins Sheet 5

Date of works: Start time of works: End time of works: (24 hour time)

Issue	Desg	Appd	Date & Time	Amendment Description	TGS Name & Number:	TGS Designed By:	PWZTMP:	Exp:	N/A	Signature:	Date of Approval:	Page 4 / 6
01	KD	PL	01.09.23 - 0702	Original Issue	LGP - TGS - 49876 - Nelson St & Pacific Hwy, Chatswood NSW 2067, Australia - 003 - R4	TGS Approved By:	Peter Lozano	PWZTMP:	TCT0058486	Exp:	N/A	
02	PL	KD	06.09.23 - 1408	Amended as requested	Works Location:	Client Company:	Systems Connect	Client:	Systems Connect	Signature:		
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04	PL	KD	28.09.23 - 0842	Amended as requested	Project Name:	Southwest Line-wide Works	Project Description:	1 of 3 lane Takeout - Footpath Closure				
05	KD	PI	10.10.23 - 1:55am	Amended as requested								
Scale: 1 : 750			Original Size A3		Lack Group acknowledges the traditional owners of country throughout Australia and recognises their continuing connection to land, waters and community. We pay our respect to them and their cultures; and to elders both past and present.							





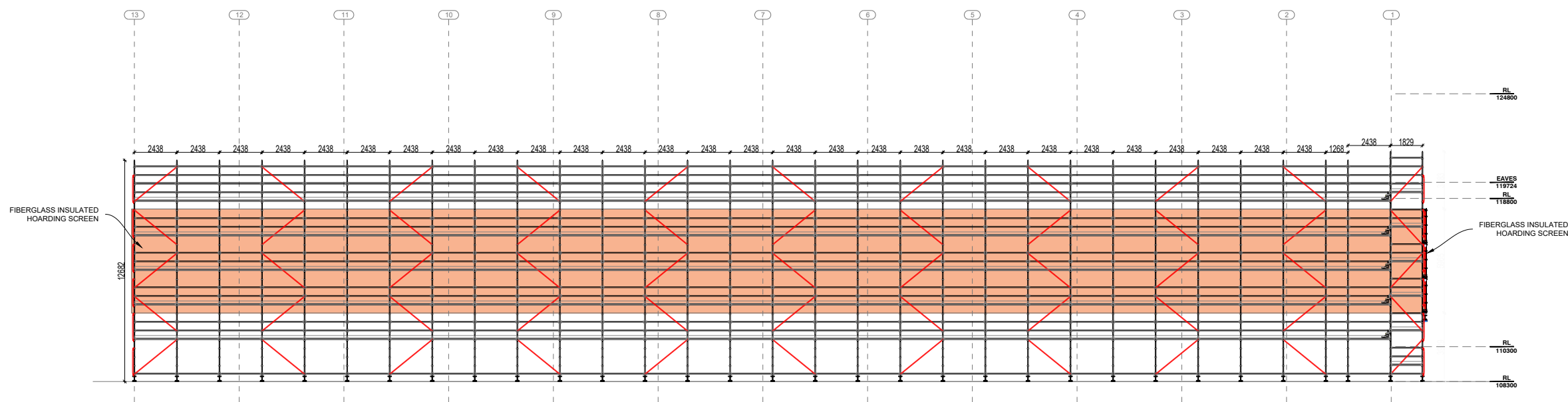


HEAVY DUTY (675 kg) IS APPLIED ON ONE WORKING DECK PER BAY.



01
08

PLAN VIEW



02
08

NORTH ELEVATION

- One working platform per bay
- Heavy duty 6.6kN/bay, includes one 2 kN concentrated load, Min 900mm platform
- Medium duty 4.4kN/bay, includes one 1.5 kN concentrated load, Min 675mm platform
- Light duty 2.2kN/bay, includes one 1.2 kN concentrated load, Min 450mm platform
- Load action on Hop-ups is limited to light duty loading
- A clear access of not less than 450mm shall be maintained on all platforms
- Clear headroom shall be a minimum of 1.85m
- Loading platform rating indicates overall load per one working platform at a time.

- Refer to Safe Work NSW & Australian Standards Guidelines for safe use of scaffold
- This design relates to components supplied by the scaffolding company, named below
- Consult with Kingle Designer for any changes
- Signage and higher solidity cladding require additional measures
- Soleboard (Min 0.5mx0.225m) to be used where required to distribute the load
- It is the Client's responsibility to provide adequate foundation/ supporting structure/ compacted soil
- Avoid undermining or excavation in the vicinity of the scaffold
- DO NOT alter the scaffold or remove the ties without permission from Kingle Pty Ltd
- Where required, it is the client's responsibility to provide vehicle impact protection
- Typical imposed working loads of up to 6kN horizontal per tie, and up to 20kN vertical load per leg have been considered in the design
- Nominal gap from scaffold platform to building face 100mm to 225mm unless internal edge protection is provided
- Max 4m of scaffold is permitted above the highest row of ties
- Max 2m of shade cloth is permitted above the highest row of ties
- Screw-tightened couplers, should be tightened to a torque of 60Nm
- Scaffolds of 3m lift should be boxed at 2m height
- Load limit on drilled-in anchors 6kN
- When screwing concrete screws always use the lowest setting on the drill
- Refer to the below diagrams for ties and braces setup
- This is not a final works as executed drawing, site conditions may require this drawing to change.
- These drawings are not certification of design as built.
- Engineer certificates are required for each engineered scaffold prior to use.

[illegible]

REVISION	DESCRIPTION	DATE
REVISION 00	INITIAL DESIGN	07/09/2023
REVISION 01	CHANGING DESIGN	18/09/2023

KINGLE PTY LTD



UNLESS OTHERWISE SPECIFIED:
DIMENSIONS ARE IN MILLIMETERS

SCAFFOLDING COMPANY:
BAYSIDE SCAFFOLDING SERVICES

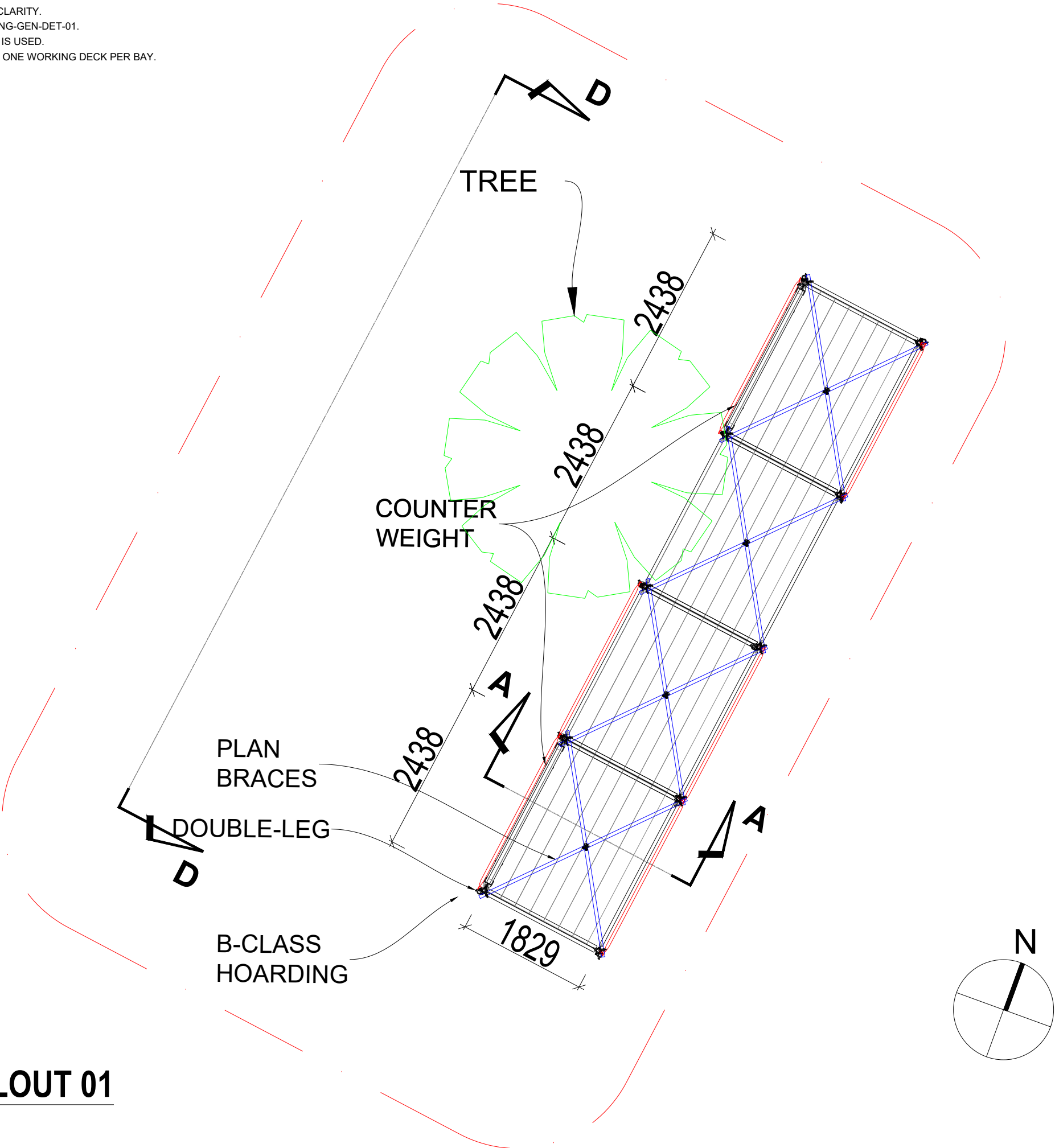
REVISION 01

CLIENT: PERFECT CONTRACTING PTY LIMITED

TITLE: PERIMETER SCAFFOLD
ADDRESS: 607 PACIFIC HIGHWAY, CHATSWOOD

	NAME	DATE
DRAWN	A.L.	18/09/2023
APPROVED	B.H.	18/09/2023
DRAWING NO.: BAY-PRC-6289-01		A3
DATE: 18/09/2023		SHEET:01/05

NOTE: SOME DETAILS ARE LEFT OUT FOR CLARITY.
FOR GENERIC DETAILS REFER TO KNG-GEN-DET-01.
KWKSTAGE SCAFFOLDING SYSTEM IS USED.
HEAVY DUTY (675 kg) IS APPLIED ON ONE WORKING DECK PER BAY.



03
08

CALLOUT 01

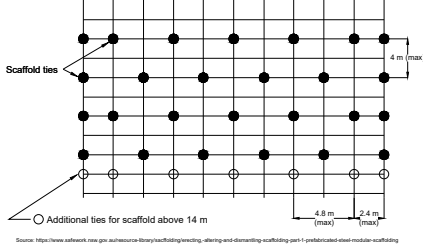
Scaffold Details (AS/NZS 1576.1):

- One working platform per bay
- Heavy duty 6.6kN/bay, includes one 2 kN concentrated load, Min 900mm platform
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- Load action on Hop-ups is limited to light duty loading
- A clear access of not less than 450mm shall be maintained on all platforms
- Clear headroom shall be a minimum of 1.85m
- Loading platform rating indicates overall load per one working platform at a time.

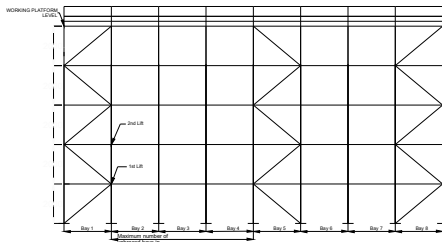
General Notes: (Notes on drawings supersede these general notes)

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Typical Horizontal and Vertical Tie Spacing



Typical Heel-and-Toe Bracing of Bays (AS/NZS 1576.6)



REVISION	DESCRIPTION	DATE
REVISION 00	INITIAL DESIGN	07/09/2023
REVISION 01	CHANGING DESIGN	18/09/2023

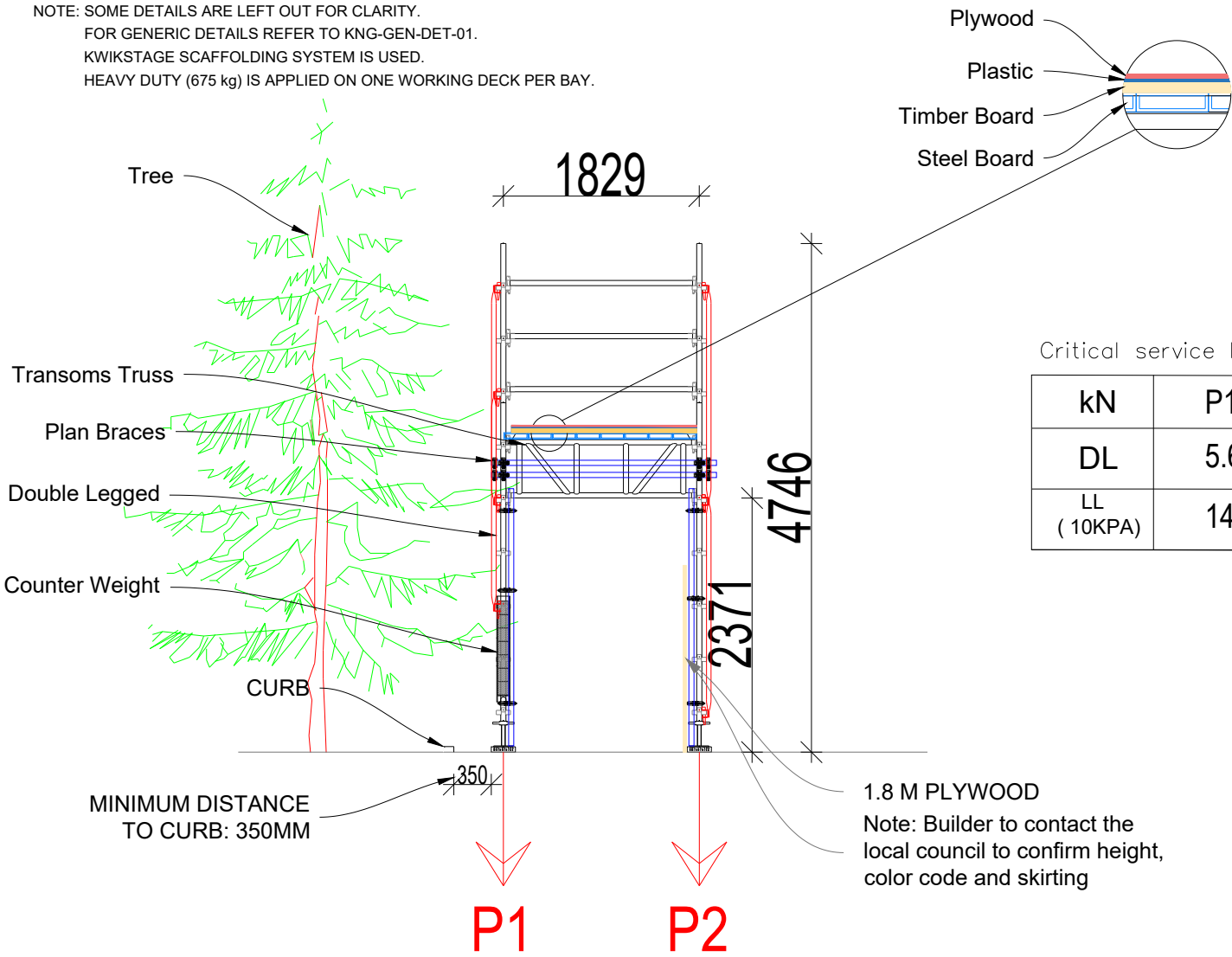
KINGLE PTY LTD

**KINGLE**

UNLESS OTHERWISE SPECIFIED: DIMENSIONS ARE IN MILLIMETERS	REVISION 01
SCAFFOLDING COMPANY: BAYSIDE SCAFFOLDING SERVICES	
CLIENT: PERFECT CONTRACTING PTY LIMITED	DO NOT SCALE DRAWING

TITLE: PERIMETER SCAFFOLD ADDRESS: 607 PACIFIC HIGHWAY, CHATSWOOD		
	NAME	DATE
DRAWN	A.L.	18/09/2023
APPROVED	B.H.	18/09/2023
DRAWING NO.: BAY-PRC-6289-01		A3
DATE: 18/09/2023		SHEET:02/05

NOTE: SOME DETAILS ARE LEFT OUT FOR CLARITY.
FOR GENERIC DETAILS REFER TO KNG-GEN-DET-01.
KWKSTAGE SCAFFOLDING SYSTEM IS USED.
HEAVY DUTY (675 kg) IS APPLIED ON ONE WORKING DECK PER BAY.



Critical service load per leg

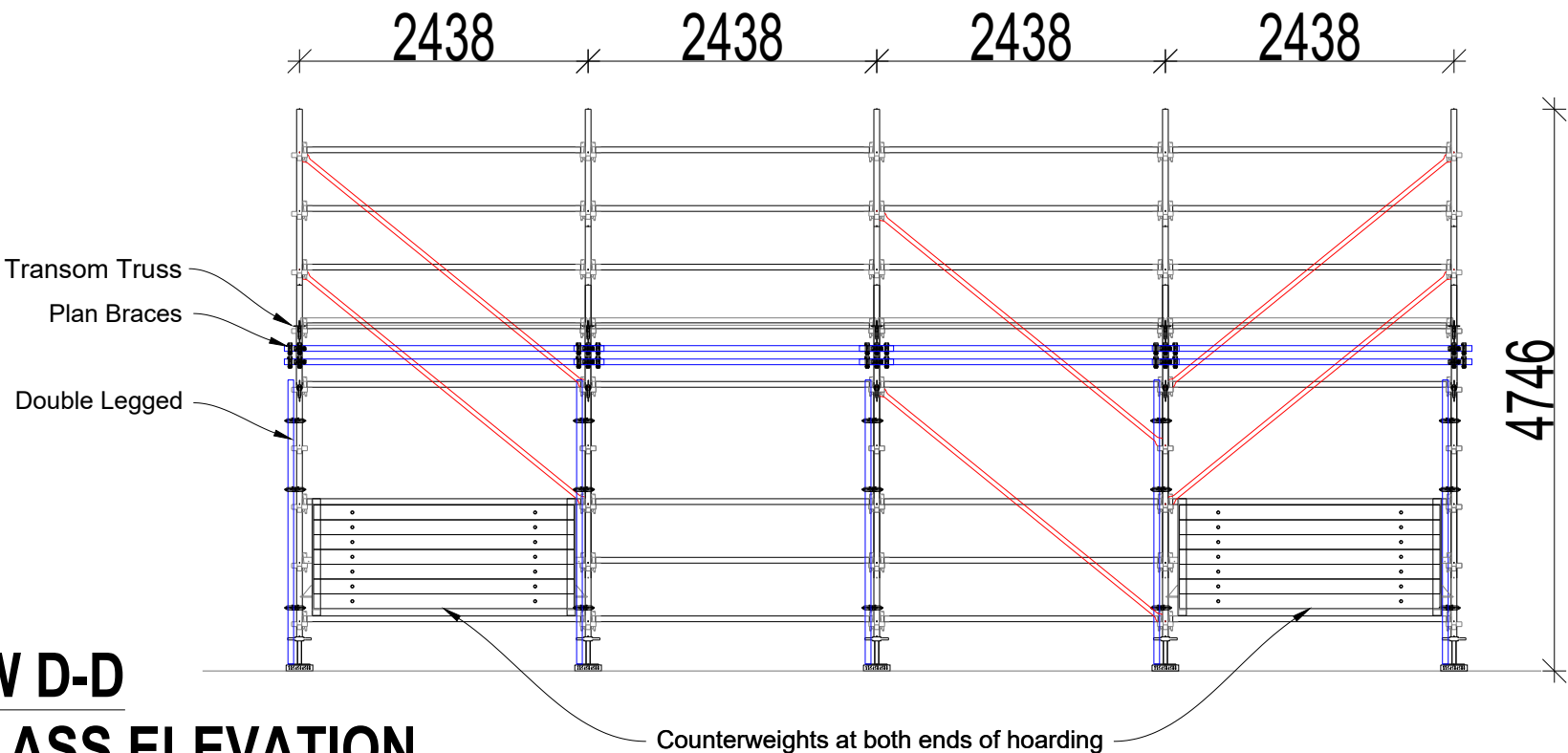
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DL	5.6	3.1
LL (10KPA)	14.4	14.4

04
08

SECTION A-A B-CLASS

05
08

VIEW D-D B-CLASS ELEVATION



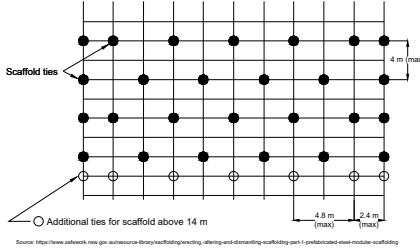
Scaffold Details (AS/NZS 1576.1):

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- A clear access of not less than 450mm shall be maintained on all platforms
- Clear headroom shall be a minimum of 1.85m
- Loading platform rating indicates overall load per one working platform at a time.

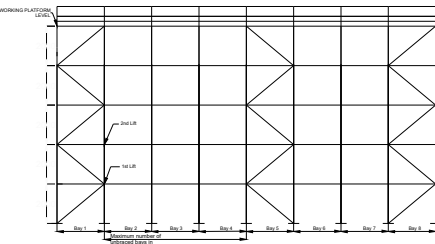
General Notes: (Notes on drawings supersede these general notes)

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Typical Horizontal and Vertical Tie Spacing



Typical Heel-and-Toe Bracing of Bays (AS/NZS 1576.6)



REVISION	DESCRIPTION	DATE
REVISION 00	INITIAL DESIGN	07/09/2023
REVISION 01	CHANGING DESIGN	18/09/2023

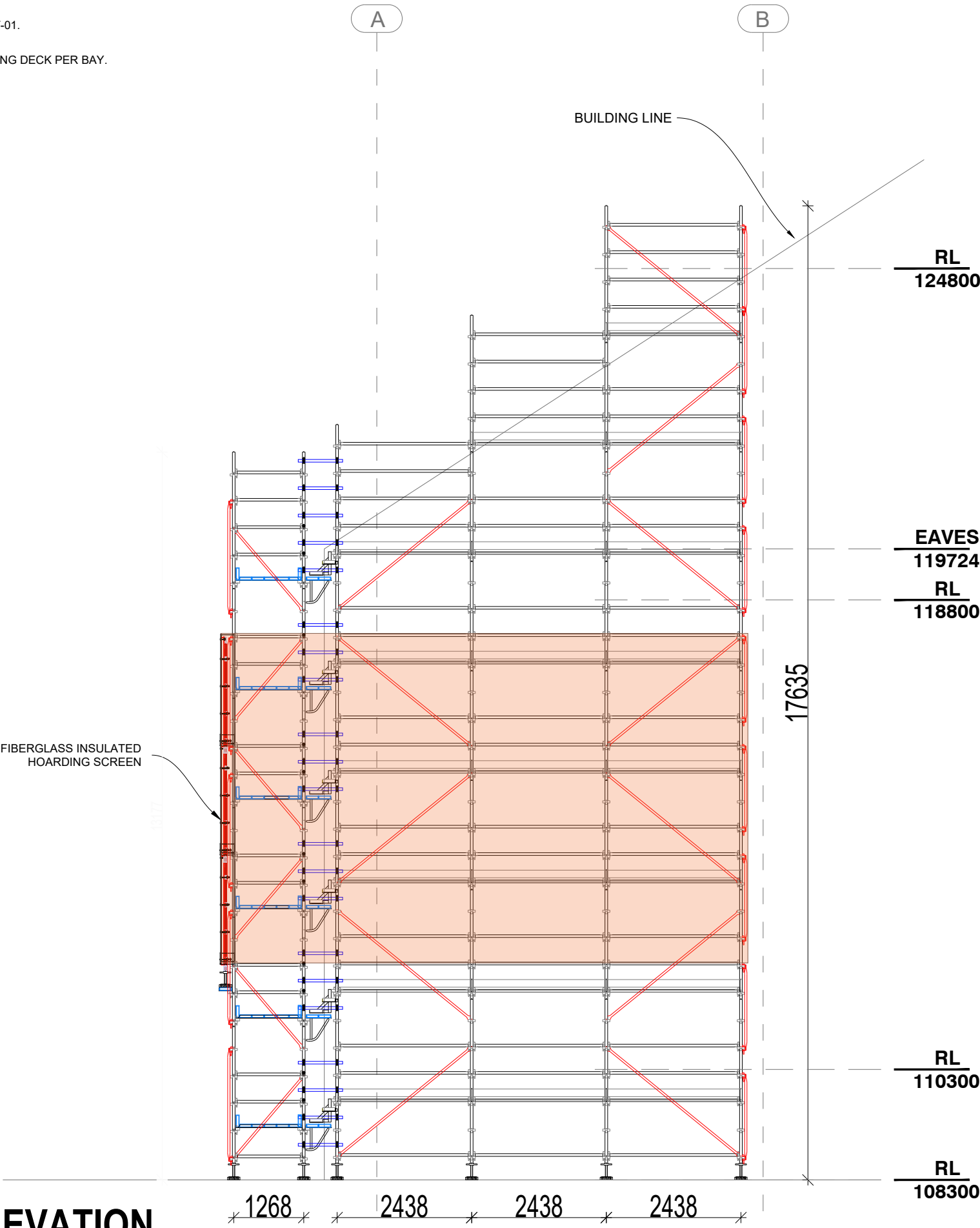
KINGLE PTY LTD



UNLESS OTHERWISE SPECIFIED: DIMENSIONS ARE IN MILLIMETERS	REVISION 01
SCAFFOLDING COMPANY: BAYSIDE SCAFFOLDING SERVICES	
CLIENT: PERFECT CONTRACTING PTY LIMITED	DO NOT SCALE DRAWING

TITLE: PERIMETER SCAFFOLD ADDRESS: 607 PACIFIC HIGHWAY, CHATSWOOD		
	NAME	DATE
DRAWN	A.L.	18/09/2023
APPROVED	B.H.	18/09/2023
DRAWING NO.: BAY-PRC-6289-01		A3
DATE: 18/09/2023		SHEET:03/05

NOTE: SOME DETAILS ARE LEFT OUT FOR CLARITY.
FOR GENERIC DETAILS REFER TO KNG-GEN-DET-01.
KWKSTAGE SCAFFOLDING SYSTEM IS USED.
HEAVY DUTY (675 kg) IS APPLIED ON ONE WORKING DECK PER BAY.



FIBERGLASS INSULATED
HOARDING SCREEN

17635

RL
124800

EAVES
119724
RL
118800

RL
110300

RL
108300

1268 2438 2438 2438

06
08 WEST ELEVATION

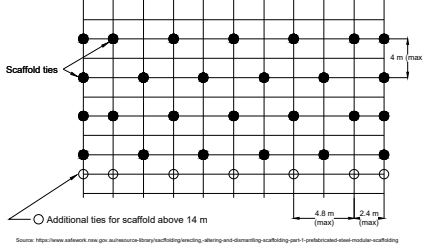
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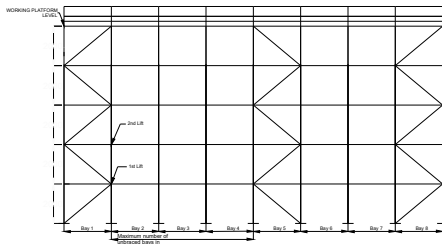
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REVISION	DESCRIPTION	DATE
REVISION 00	INITIAL DESIGN	07/09/2023
REVISION 01	CHANGING DESIGN	18/09/2023

KINGLE PTY LTD

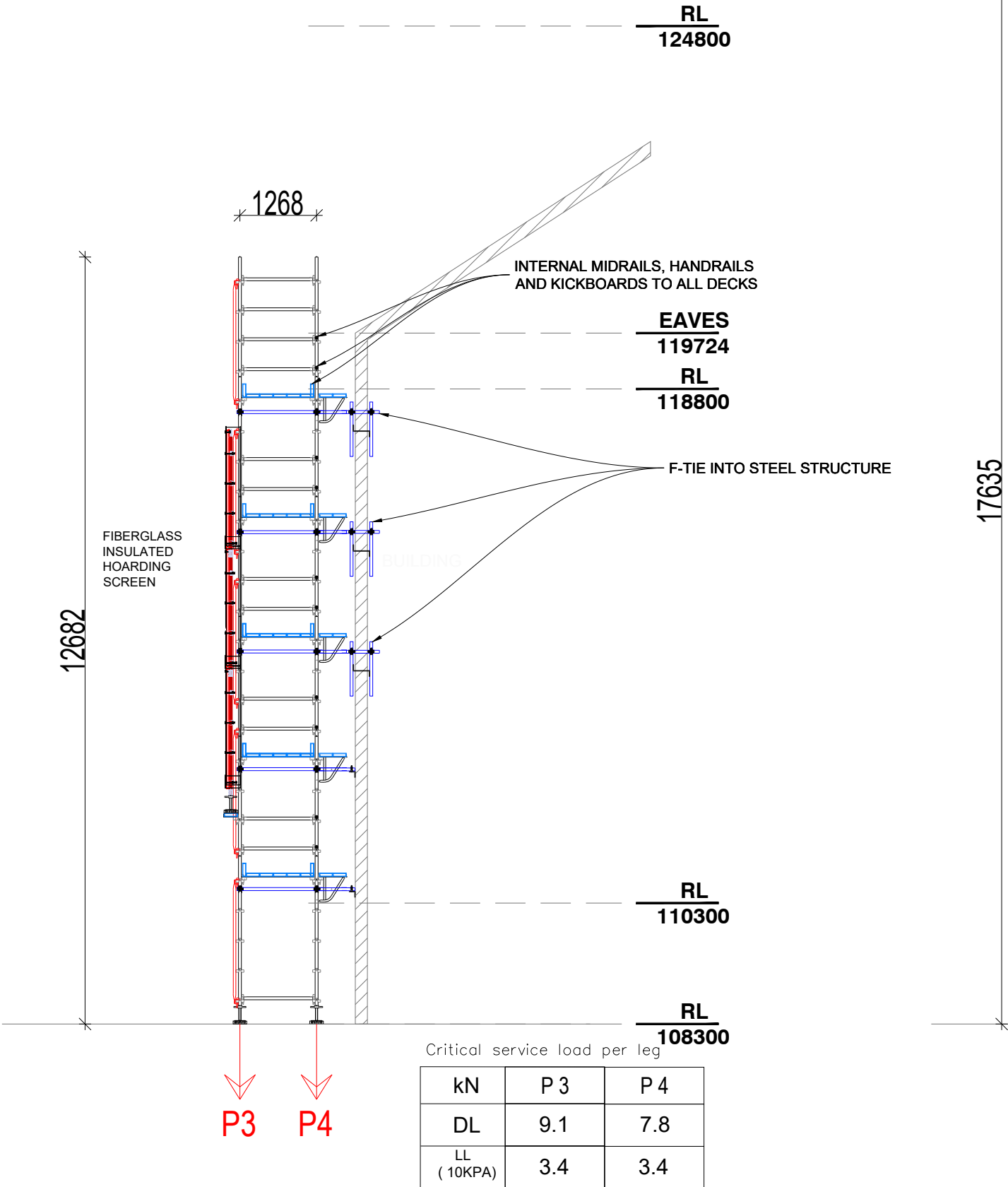


KINGLE

UNLESS OTHERWISE SPECIFIED: DIMENSIONS ARE IN MILLIMETERS	REVISION 01
SCAFFOLDING COMPANY: BAYSIDE SCAFFOLDING SERVICES	
CLIENT: PERFECT CONTRACTING PTY LIMITED	DO NOT SCALE DRAWING

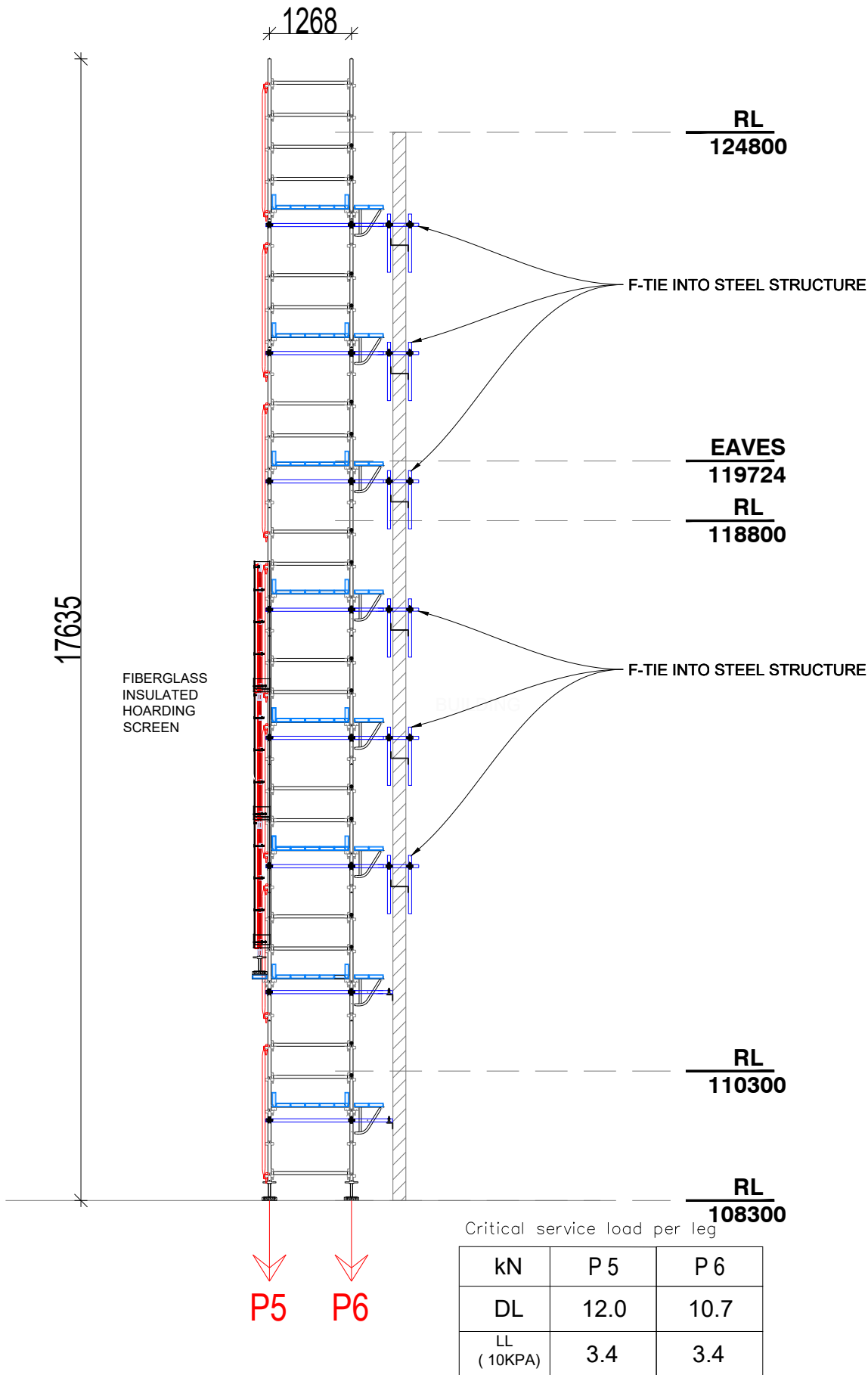
TITLE: PERIMETER SCAFFOLD ADDRESS: 607 PACIFIC HIGHWAY, CHATSWOOD		
	NAME	DATE
DRAWN	A.L.	18/09/2023
APPROVED	B.H.	18/09/2023
DRAWING NO.: BAY-PRC-6289-01		A3
DATE: 18/09/2023		SHEET:04/05

NOTE: SOME DETAILS ARE LEFT OUT FOR CLARITY.
FOR GENERIC DETAILS REFER TO KNG-GEN-DET-01.
KWKSTAGE SCAFFOLDING SYSTEM IS USED.
HEAVY DUTY (675 kg) IS APPLIED ON ONE WORKING DECK PER BAY.



07
08

SECTION B-B



08
08

SECTION C-C

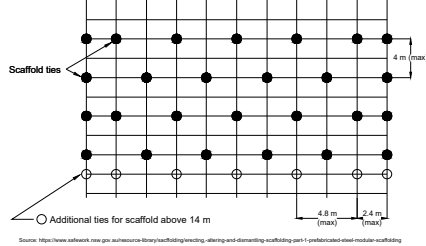
Scaffold Details (AS/NZS 1576.1):

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- Light duty 2.2kN/bay, includes one 1.2 kN concentrated load, Min 450mm platform
- Load action on Hop-ups is limited to light duty loading
- A clear access of not less than 450mm shall be maintained on all platforms
- Clear headroom shall be a minimum of 1.85m
- Loading platform rating indicates overall load per one working platform at a time.

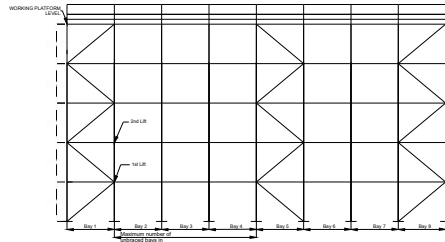
General Notes: (Notes on drawings supersede these general notes)

- Refer to Safe Work NSW & Australia Standards Guidelines for safe use of scaffold
- This design relates to components supplied by the scaffolding company, named below
- Consult with Kingle designer for any changes
- Signage and higher solidity cladding require additional measures
- Soleboard (Min 0.5mx0.225m) to be used where required to distribute the load
- It is the Client's responsibility to provide adequate foundation/ supporting structure/ compacted soil
- Avoid undermining or excavation in the vicinity of the scaffold
- DO NOT alter the scaffold or remove the ties without permission from Kingle Pty Ltd
- Where required, it is the client's responsibility to provide vehicle impact protection
- Typical imposed working loads of up to 6kN horizontal per tie, and up to 20kN vertical load per leg have been considered in the design
- Nominal gap from scaffold platform to building face 100mm to 225mm unless internal edge protection is provided
- Max 4m of scaffold is permitted above the highest row of ties
- Max 2m of shade cloth is permitted above the highest row of ties
- Screw-tightened couplers, should be tightened to a torque of 60Nm
- Scaffolds of 3m lift should be boxed at 2m height
- Load limit on drilled-in anchors 6kN
- When screwing concrete screws always use the lowest setting on the drill
- Refer to the below diagrams for ties and braces setup
- This is not a final works as executed drawing, site conditions may require this drawing to change.
- These drawings are not certification of design as built.
- Engineer certificates are required for each engineered scaffold prior to use.

Typical Horizontal and Vertical Tie Spacing



Typical Heel-and-Toe Bracing of Bays (AS/NZS 1576.6)



REVISION	DESCRIPTION	DATE
REVISION 00	INITIAL DESIGN	07/09/2023
REVISION 01	CHANGING DESIGN	18/09/2023

KINGLE PTY LTD

**KINGLE**

UNLESS OTHERWISE SPECIFIED:
DIMENSIONS ARE IN MILLIMETERS

SCAFFOLDING COMPANY:
BAYSIDE SCAFFOLDING SERVICES

CLIENT: PERFECT CONTRACTING PTY LIMITED

DO NOT SCALE DRAWING

TITLE: PERIMETER SCAFFOLD
ADDRESS: 607 PACIFIC HIGHWAY, CHATSWOOD

	NAME	DATE
DRAWN	A.L.	18/09/2023
APPROVED	B.H.	18/09/2023
DRAWING NO.: BAY-PRC-6289-01		A3
DATE: 18/09/2023		SHEET:05/05

1.8.4.1 Willoughby Council Consultation

Keswick, Matthew

From: Sim, Adeline <Adeline.Sim@Willoughby.nsw.gov.au>
Sent: Tuesday, 17 October 2023 11:09 AM
To: Keswick, Matthew
Cc: Sui, Daniel; Gill, John; Sim, Mong; Austin, Jason
Subject: RE: Teambinder Update - Northern Dive CTMP Update

CAUTION: This email originated from outside of the Organisation.

Hi Matthew,

All good now for the TGS, please ensure the adjacent residents are notified prior starting the work. I have also included the comments/ requirements from our Tree Section:

The Arboricultural Assessment undertaken by Urban Tree Management, dated 15 October 2023, has been reviewed. The proposed pruning to facilitate the installation of the hording and to ensure crown protection for both trees equates to less than 10% of each individual trees crown. The tree section has no objection to the proposed pruning subject to the following:

- The works are to be undertaken as per the recommendations in the Arboricultural Assessment.
- The works are to be completed at Sydney Metro's cost by a minimum AQF Level 3 Arborist.

Regards,
Adeline

From: Keswick, Matthew <Matthew.Keswick@sclww.com.au>
Sent: Tuesday, 17 October 2023 10:45 AM
To: Sim, Adeline <Adeline.Sim@Willoughby.nsw.gov.au>
Cc: Sui, Daniel <Daniel.Sui@Willoughby.nsw.gov.au>; Gill, John <John.Gill@Willoughby.nsw.gov.au>; Sim, Mong <Mong.Sim@cpbdownerjv.com.au>; Austin, Jason <Jason.Austin@sclww.com.au>
Subject: RE: Teambinder Update - Northern Dive CTMP Update

Hi Adeline,

Thanks for your response. The lane closure on Pacific Highway will be assessed by TfNSW at the time we apply for the ROL

We will cover the requirement for a VMS board at this time

Regards,

Matt

From: Sim, Adeline <Adeline.Sim@Willoughby.nsw.gov.au>
Sent: Monday, October 16, 2023 7:58 AM
To: Keswick, Matthew <Matthew.Keswick@sclww.com.au>
Cc: Sui, Daniel <Daniel.Sui@Willoughby.nsw.gov.au>; Gill, John <John.Gill@Willoughby.nsw.gov.au>; Sim, Mong <Mong.Sim@cpbdownerjv.com.au>; Austin, Jason <Jason.Austin@sclww.com.au>
Subject: RE: Teambinder Update - Northern Dive CTMP Update

CAUTION: This email originated from outside of the Organisation.

Hi Matt,

The CTPM doesn't go in to traffic committee.

The VMS wouldn't need for the closure of Nelson St. I was referring to the segment of lane closure along Pacific Hwy that needs VMS. Has that been separately assessed and advised by TfNSW?

Regards,
Adeline

From: Keswick, Matthew <Matthew.Keswick@sclww.com.au>

Sent: Friday, 13 October 2023 3:55 PM

To: Sim, Adeline <Adeline.Sim@Willoughby.nsw.gov.au>

Cc: Sui, Daniel <Daniel.Sui@Willoughby.nsw.gov.au>; Gill, John <John.Gill@Willoughby.nsw.gov.au>; Sim, Mong <Mong.Sim@cpbdownerjv.com.au>; Austin, Jason <Jason.Austin@sclww.com.au>

Subject: RE: Teambinder Update - Northern Dive CTMP Update

Hi Adeline,

Thanks for that. Please see attached response we have had from the buses.

Regarding the VMS boards, we believe a VMS is not necessary as Nelson Street is a dead end road and we will separately carry out notification of residents through our communications team

Also, could you please confirm that these works do not need to go in front of the traffic committee?

Thanks for your help

Regards,

Matt

From: Sim, Adeline <Adeline.Sim@Willoughby.nsw.gov.au>

Sent: Friday, October 13, 2023 12:12 PM

To: Keswick, Matthew <Matthew.Keswick@sclww.com.au>

Cc: Sui, Daniel <Daniel.Sui@Willoughby.nsw.gov.au>; Gill, John <John.Gill@Willoughby.nsw.gov.au>

Subject: RE: Teambinder Update - Northern Dive CTMP Update

CAUTION: This email originated from outside of the Organisation.

Hi Matthew,

TGS looks fine. Do you plan to deploy VMS? If so please indicate your plan and position for locations for VMS. Can you please inform bus operators on the date and duration, and let them know the arrangement near bus stops around the lane closure area.

Regards,
Adeline

Adeline Sim - Senior Transport Engineer

WILLOUGHBY CITY COUNCIL

PO Box 57 Chatswood NSW 2057

P +61 2 9777 7705 | M +61409907678

E Adeline.Sim@Willoughby.nsw.gov.au
willoughby.nsw.gov.au | visitchatswood.com.au | theconcourse.com.au

Council acknowledges the Gamaragal People as the Traditional Owners of these lands. We pay our respects to their Elders past and present.



From: Sui, Daniel <Daniel.Sui@Willoughby.nsw.gov.au>
Sent: Tuesday, 10 October 2023 4:47 PM
To: Sim, Adeline <Adeline.Sim@Willoughby.nsw.gov.au>; Gill, John <John.Gill@Willoughby.nsw.gov.au>
Cc: Matthew.Keswick@sclww.com.au
Subject: Fw: Teambinder Update - Northern Dive CTMP Update

Hi Adeline

Can you please review the CTMPs and provide comments to Matthew

John - what is the process for Out of hours application ?

Thanks,
Daniel

Daniel Sui - Traffic and Transport Team Lead
WILLOUGHBY CITY COUNCIL
PO Box 57 Chatswood NSW 2057
P +61 2 9777 7750 | M +61409785819
E Daniel.Sui@Willoughby.nsw.gov.au
willoughby.nsw.gov.au | visitchatswood.com.au | theconcourse.com.au



Council acknowledges the Gamaragal People as the Traditional Owners of these lands. We pay our respects to their Elders past and present.

From: Keswick, Matthew <Matthew.Keswick@sclww.com.au>
Sent: Tuesday, 10 October 2023 4:24 PM
To: Sui, Daniel <Daniel.Sui@Willoughby.nsw.gov.au>
Cc: Arbabi, Kiavash <Kiavash.Arbabi@sclww.com.au>; Austin, Jason <Jason.Austin@sclww.com.au>
Subject: RE: Teambinder Update - Northern Dive CTMP Update

Hi Daniel,

Please see attached proposed TCPs that relate to the works on Nelson Street, these are in the process of being updated but they show the allowances for cyclists and for the parking on the northern side of Nelson Street

For the proposed TCP for work at night on the Pacific Highway we can accommodate pedestrians and dismounted cyclists by having a traffic controller escort them through the work area

Could you please let me know if this answers the concerns and if so, I'll update the CTMP accordingly

Thanks for your help

Regards,

Matt

From: Keswick, Matthew

Sent: Wednesday, September 20, 2023 12:19 PM

To: Sui, Daniel <Daniel.Sui@Willoughby.nsw.gov.au>

Cc: Arbabi, Kiavash <Kiavash.Arbabi@sclww.com.au>; Austin, Jason <Jason.Austin@sclww.com.au>

Subject: FW: Teambinder Update - Northern Dive CTMP Update

Hi Daniel,

Please see below link to the updated CTMP for the Sydney Metro Northern Dive Site in Chatswood

My apologies your name wasn't on the transmittal list last week

 [TMP Northern Dive Site Operations Rev 4-KA.pdf](#)

Regards,

Matt

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1.8.4.2 Sydney Buses Consultation

Keswick, Matthew

From: Frankie Passarelli <Frankie.PASSARELLI@transport.nsw.gov.au>
Sent: Tuesday, 10 October 2023 2:19 PM
To: Keswick, Matthew
Cc: Sim, Mong; Elizabeth Harrison; Jim Niahos; Steven Blight; Rabih Bekdache; James Makasiale; Ben Cantor
Subject: RE: Systems Connect - Northern Dive Service Buildings - Upcoming Night Works on Pacific Highway Chatswood
Attachments: LGP - TGS - 49876 - Nelson St & Pacific Hwy, Chatswood NSW 2067, Australia - 003 - R5.pdf

CAUTION: This email originated from outside of the Organisation.

Matt,

Thanks for the email.

As per Rabih's initial advice, I have no objections to :

- Night works as per setup attached :
 - Sunday to Thursday 2200 – 0500;
 - no works Friday or Saturday nights
- No works during special events (VIVID et al)

I have included the local operator representatives for any final commentary.

Regards,

Frankie Passarelli

A/ Manager Alternate Transport Planning

Transport Planning Project Manager

Customer Journey Planning – Short Term & Temporary Transport Planning

Greater Sydney | **Transport for NSW**

E: Frankie.PASSARELLI@transport.nsw.gov.au

T: 0447 174 312

231 Elizabeth St | Sydney NSW 2000



I acknowledge the Aboriginal people of the country on which I work, their traditions, culture and a shared history and identity. I also pay my respects to Elders past and present and recognise the continued connection to country.

Please consider the environment before printing this email.

OFFICIAL

From: Keswick, Matthew <Matthew.Keswick@sclww.com.au>

Sent: Tuesday, 10 October 2023 1:45 PM

To: Frankie Passarelli <Frankie.PASSARELLI@transport.nsw.gov.au>

Cc: Sim, Mong <Mong.Sim@cpbdownerjv.com.au>; Elizabeth Harrison <Elizabeth.Harrison@transport.nsw.gov.au>; Jim Niahos <Jim.Niahos@transport.nsw.gov.au>; Steven Blight <Steven.Blight2@transport.nsw.gov.au>; Rabih Bekdache <Rabih.Bekdache@transport.nsw.gov.au>

Subject: RE: Systems Connect - Northern Dive Service Buildings - Upcoming Night Works on Pacific Highway Chatswood

You don't often get email from matthew.keswick@sclww.com.au. [Learn why this is important](#)

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Hi Frankie,

We have taken a closer look at the controls on the Pacific Highway to try to reduce our impacts on everyone

We have marked up the location of the bus stop 206747 on the Pacific Highway and can confirm that our works will not affect it

I also confirm that the hoarding will be minimum 800mm set back from the kerb

If acceptable, could you please confirm this any and conditions required

Thanks for your help

Regards,

Matt

OFFICIAL

From: Rabih Bekdache <Rabih.Bekdache@transport.nsw.gov.au>

Sent: Wednesday, October 4, 2023 1:21 PM

To: Keswick, Matthew <Matthew.Keswick@sclww.com.au>

Cc: Frankie Passarelli <Frankie.PASSARELLI@transport.nsw.gov.au>; Sim, Mong <Mong.Sim@cpbdownerjv.com.au>; Elizabeth Harrison <Elizabeth.Harrison@transport.nsw.gov.au>; Jim Niahos <Jim.Niahos@transport.nsw.gov.au>; Steven Blight <Steven.Blight2@transport.nsw.gov.au>

Subject: RE: Systems Connect - Northern Dive Service Buildings -

CAUTION: This email originated from outside of the Organisation.

Hello Mathew

No issues with the bus zone being blocked.

Please confirm dates so we can advise the operators.

Regards

Rabih Bekdache

Transport Planning Project Manager

Customer Journey Planning – Short Term & Temporary Transport Planning

Transport for NSW

M 0415 472 898 E rabih.bekdache@transport.nsw.gov.au

transport.nsw.gov.au

231 Elizabeth Street
Sydney NSW 2000



**Transport
for NSW**



I acknowledge the Aboriginal people of the country on which I work, their traditions, culture and a shared history and identity. I also pay my respects to Elders past and present and recognise the continued connection to country.

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OFFICIAL

From: Rabih Bekdache

Sent: Wednesday, 4 October 2023 10:22 AM

To: 'Keswick, Matthew' <Matthew.Keswick@sclww.com.au>

Cc: Frankie Passarelli <Frankie.PASSARELLI@transport.nsw.gov.au>; Sim, Mong <Mong.Sim@cpbdownerjv.com.au>;

Elizabeth Harrison <Elizabeth.Harrison@transport.nsw.gov.au>; Jim Niahos <Jim.Niahos@transport.nsw.gov.au>;

Rabih Bekdache <Rabih.Bekdache@transport.nsw.gov.au>; Steven Blight <Steven.Blight2@transport.nsw.gov.au>

Subject: RE: Systems Connect - Northern Dive Service Buildings -

Hello Keswick

I have no issues with the works and lane closure with the following conditions

- Works between Sun to Thur 2200 – 0500 no works Fri or Sat Night
- No works during a special event
- A minimum of 14 days' notice from a confirmed start date

With regards to the bus zone on our systems it doesn't look like its used but I have asked the local operators for advise if it is used and I will get back to you.

Also the class b Hoarding we require that it is set back a minimum of 600mm from the kerb face but prefer 800mm could you please confirm.

Regards

Rabih Bekdache

Transport Planning Project Manager

Customer Journey Planning – Short Term & Temporary Transport Planning

Transport for NSW



Transport
for NSW



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From: Keswick, Matthew <Matthew.Keswick@sclww.com.au>

Sent: Friday, 29 September 2023 9:14 AM

To: Rabih Bekdache <Rabih.Bekdache@transport.nsw.gov.au>

Cc: Frankie Passarelli <Frankie.PASSARELLI@transport.nsw.gov.au>; Sim, Mong <Mong.Sim@cpbdownerjv.com.au>

Subject: Systems Connect - Northern Dive Service Buildings -

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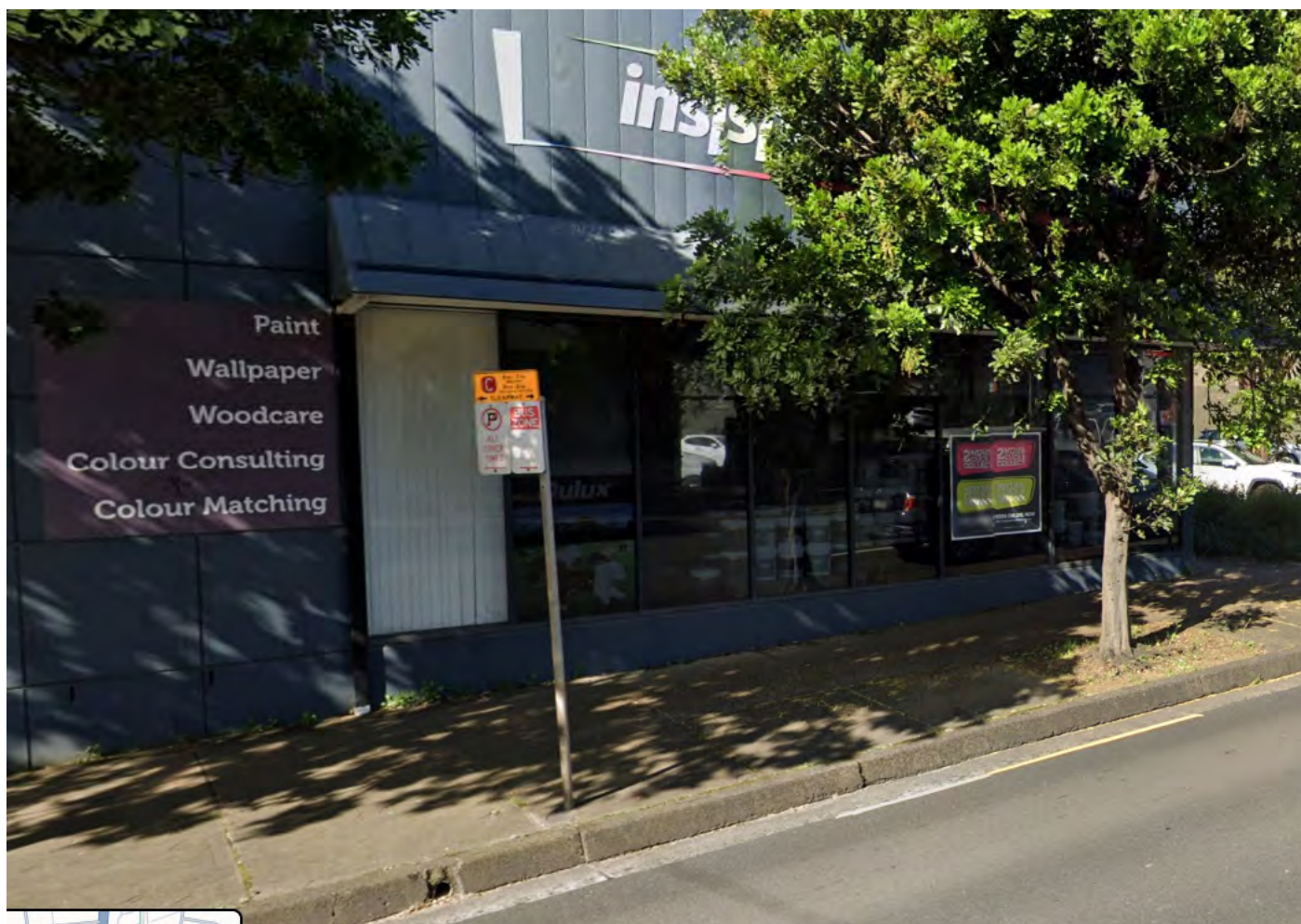
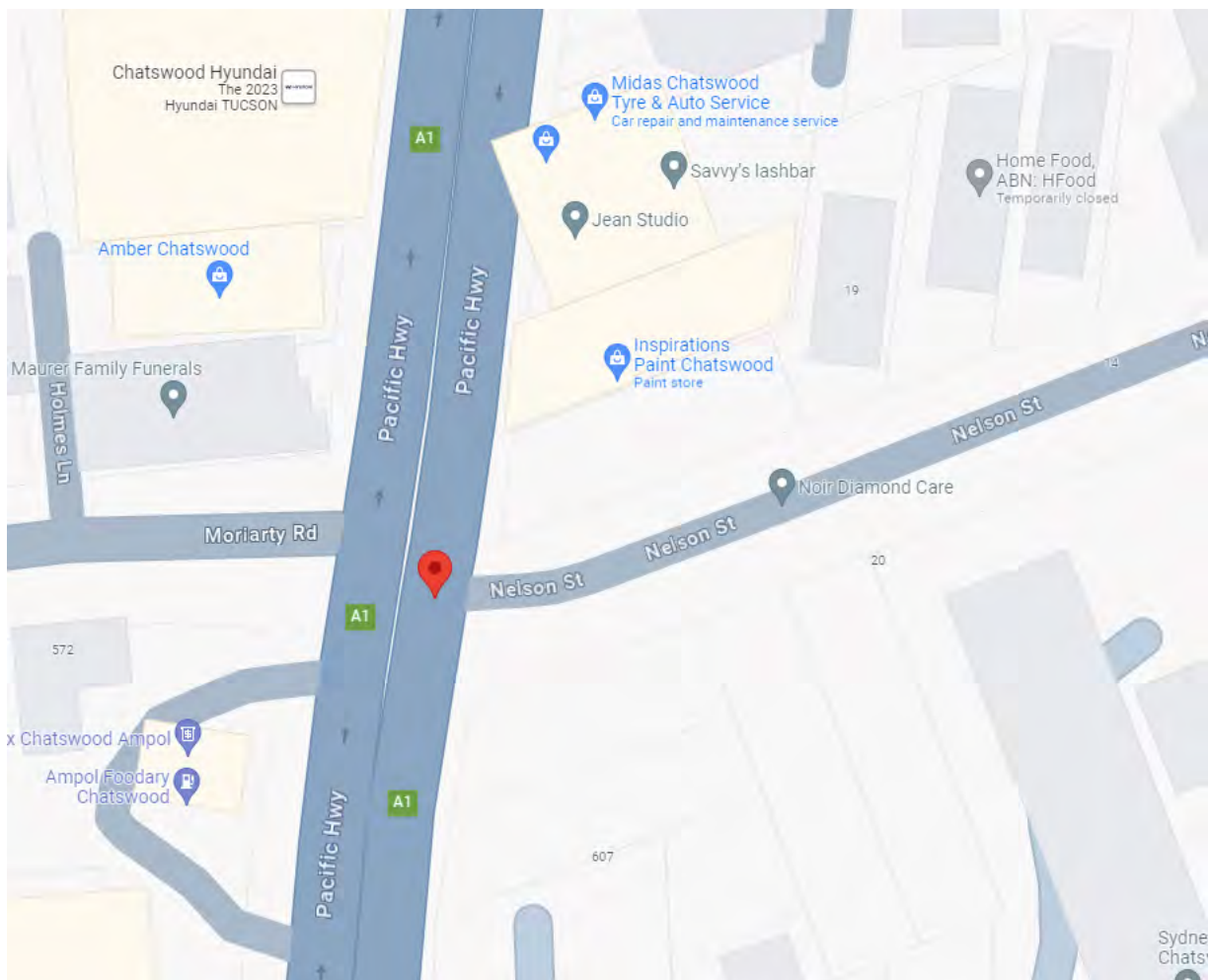
CAUTION: This email is sent from an external source. Do not click any links or open attachments unless you recognise the sender and know the content is safe.

Hi Rabih,

Please see attached proposed TCPs for Systems Connect to carry out the demolition of the redundant spoil sheds at the Sydney Metro Chatswood Dive site

One TCP is for a lane closure on Pacific Highway, this will take place at night. The day of the week and the times are TBC but we anticipate something like 10pm to 5am for a maximum of two consecutive nights for erection of class B hoarding. Two months later we will need another two consecutive nights to dismantle it

We have noticed a Bus Zone on Pacific Highway that doesn't have a bus stop attached to it (please see below screenshots from Google street view. Could you please let us know if this Bus Zone is in use during the night? We would like to position a shadow vehicle in this zone if possible



Thanks for your help

Regards,

Matt

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